

EL PASO I-10 CORRIDOR AESTHETIC MASTER PLAN



VICKI SCURI SITEWORKS 2012

EL PASO I-10 REVIEW

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EXECUTIVE SUMMARY

I-10 creates a spine through El Paso connecting the City's many neighborhoods along its length. Roadway interchanges and bridges mark each neighborhood. These bridges, whether they go under the highway, as in the neighborhoods to the east, or span over the highway, as in the downtown and to the west, serve as opportunities to represent the identity of the neighborhoods they connect. Achieving this goal requires the implementation of a comprehensive aesthetic plan including specific artistic elements. Unifying like elements makes each bridge a part of a larger scheme.

This book provides design guidelines for existing and future developments along the I-10 Corridor and serves as a reference for future roadway developments in the El Paso region. All applications are site responsive and require individual design development.

Slope Paving: eliminated in future projects; removed and replaced with rock aggregate and landscaping in existing projects where possible

Landscaping: Minimally 3 shade trees & 5 ornamentals per 1,000 sqft for gores
50 trees per acre for open space

Medians: remove hardscape; create a landscape plan with native plants and rock aggregate include identity elements

Neighborhood Identity: Use aesthetic design elements to create a sense of place

Facades: add screens inspired by local patterns and neighborhood character

Columns: add aesthetic treatments; promote pedestrian scale

Lighting: add pedestrian lighting and amenity lighting for wayfinding

Walkways: add patterns and textures; promote traffic calming

Abutments: clad with rock aggregate or paint abutments

Walls: apply penetrating stain to existing; eliminate paint and promote relief for future projects

Railings: create aesthetic railings inspired by local patterns and neighborhood identity

The City and its Agencies, with guidance from the Master Plan Design Team, determined this priority for the implementation of the design guidelines:

Airway: current

Downtown: current

Railroad Arches: current

Hawkins: future / first priority

Executive: future / second priority

Geronimo: future

Trowbridge: future

Paisano: future

Chelsea: future

Copia: future

Piedras: future

Cotton: future

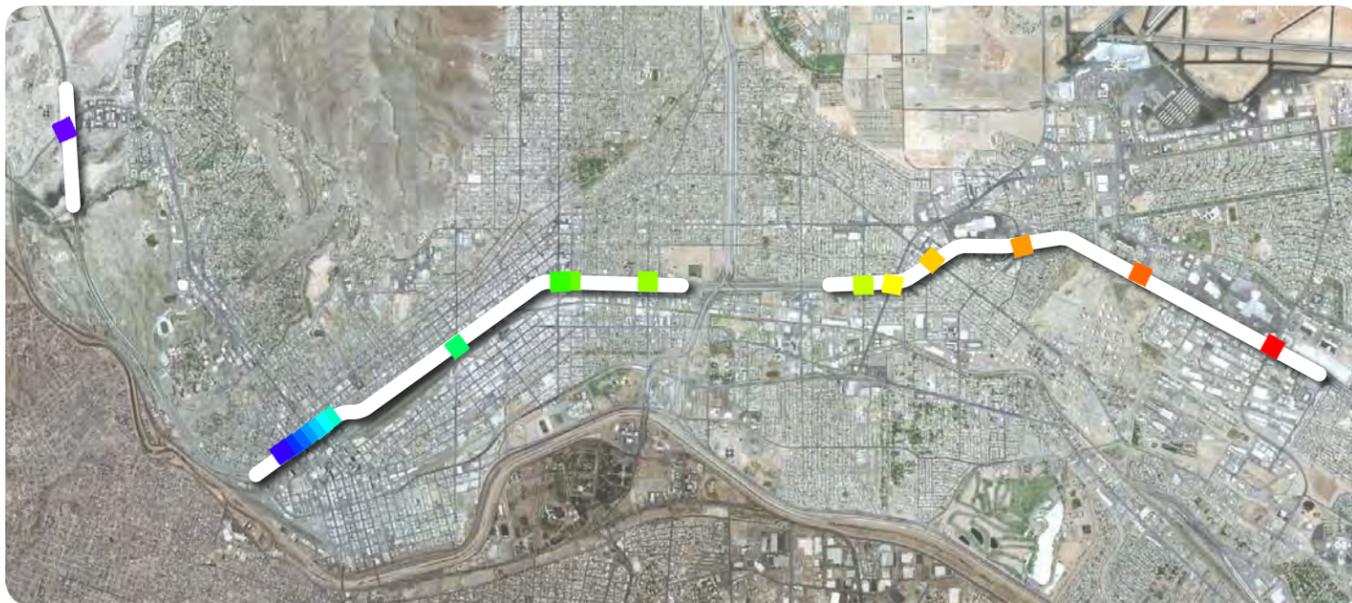
When possible, prioritize aesthetic improvements with neighborhood redevelopment. This may modify the order of implementation; take advantage of site developments as they occur over the next 15-20 years.

PART 1: SITE ANALYSIS

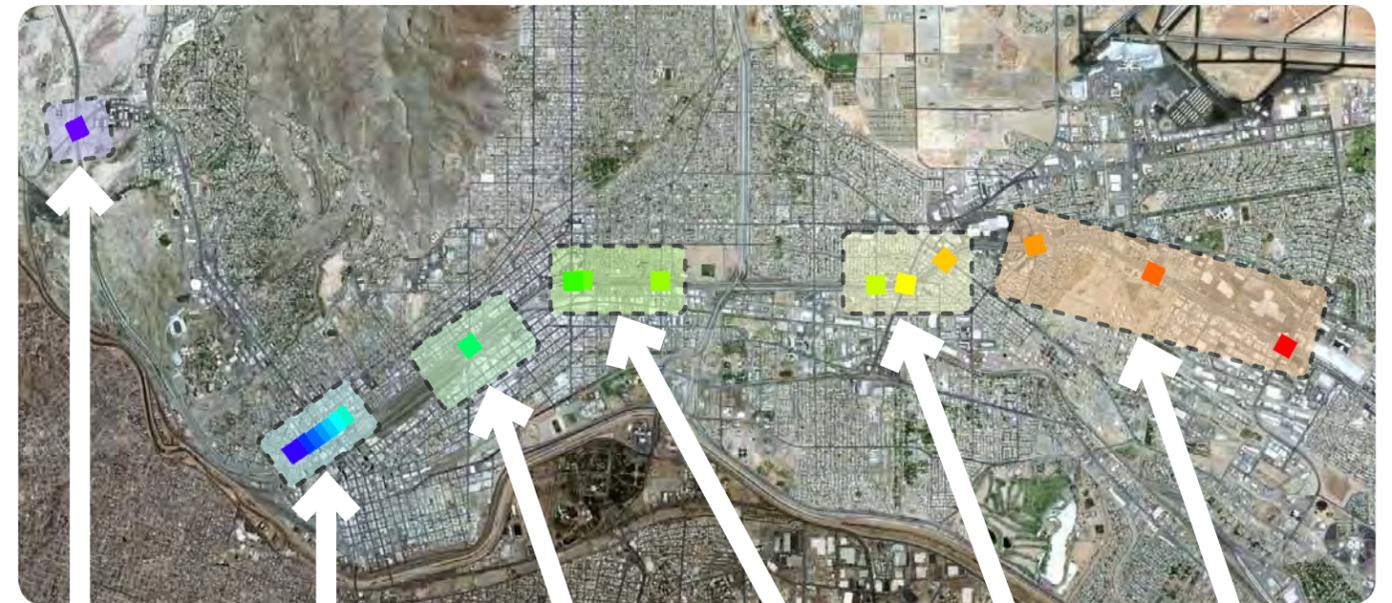
I-10 creates a spine through El Paso connecting the City's many neighborhoods along its length. Roadway interchanges and bridges mark each neighborhood. These bridges, whether they go under the highway, as in the neighborhoods to the east, or span over the highway, as in the downtown and to the west, serve as opportunities to represent the identity of the neighborhoods they connect. Achieving this goal requires the implementation of a comprehensive aesthetic plan including specific artistic elements. Unifying like elements makes each bridge a part of a larger scheme. Many of the bridges share common situations that, when addressed similarly, will achieve this unity. Each bridge must have its own distinguishing elements setting it apart from the others, creating an opportunity for unique artistic expression. This expression must reflect its neighborhood and promote a sense of place.

Each bridge exists within a specific context and neighborhood. These neighborhood contexts group according to their shared characteristics. Starting east and moving west along I-10; Hawkins, Airway and Geronimo all sit in retail areas; Trowbridge, Paisano and Chelsea group close together, sharing a part-residential/part-industrial area; Copia and Piedras share a residential neighborhood feel; Cotton occupies its own railroad district; the downtown bridges all share similar conditions crossing from the core into residential neighborhoods; and finally Executive sits isolated in the west. Grouping the bridges by their prominent neighborhood characteristics allows them to be analyzed together. This also allows the development of common approaches for similar conditions. This establishes an aesthetic hierarchy, promoting an overall vision and approach for the for the I-10 corridor, while marking districts along its length, strengthening El Paso's identity.

PROJECT SCOPE



BRIDGE GROUPINGS



Executive

Downtown
Bridges

Cotton

Copia and
Piedras

Trowbridge,
Paisano and
Chelsea

Hawkins,
Airway and
Geronimo

HAWKINS BOULEVARD

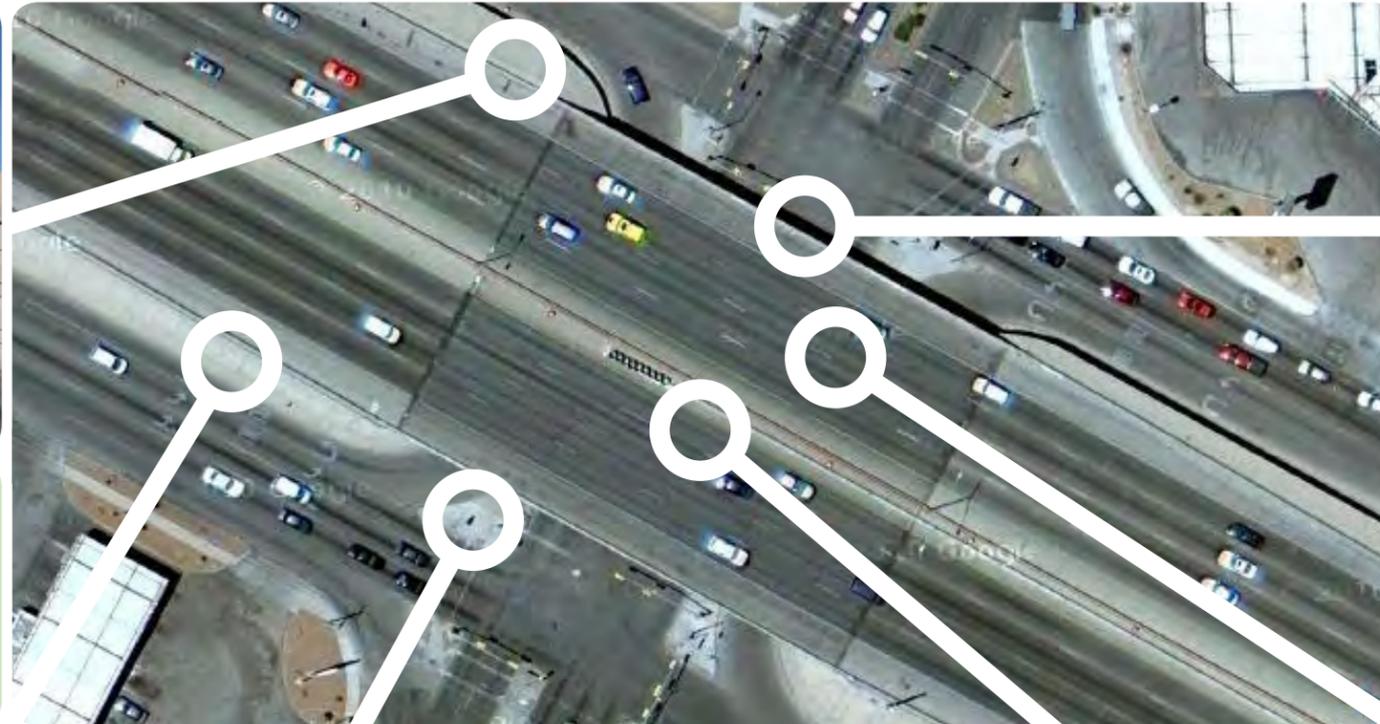
Hawkins represents an important commercial connector linking together the industrial areas to the south with the mall and residential neighborhoods to the north. As the farthest east of the bridges in the project scope it exists as a boundary condition. It introduces the themes and concepts to be applied along the entirety of the plan. Specifically, setting the level of enhancement for the neighborhoods which share retail locations and require a welcoming identity for residents and visitors alike.

Opportunities at Hawkins include landscaping; promoting pedestrian circulation; sidewalk patterning and adding elements that provide human scale; creating artistic identity features on the span and columns; and lightening the bridge's underside with paint and LED lighting. This transforms the area making it more memorable and inviting.



LANDSCAPING

Swaths of landscaping cut into the slope paving create soft rhythms of greenery with native and hardy trees and shrubs.



BRIDGE SPAN FACADE

The facade of the bridge span offers a location to implement an artistic intervention to create a sense of place.



SLOPE PAVING

The slope paving presents a monotonous and barren landscape; covering it with rock creates a visually compelling ground plane.



MEDIANS

The medians and edges of the site represent perfect locations for human scaled landscape features, making the area more welcoming.



UNDER THE BRIDGE

Both painting and light fixture choices lighten and improve the feeling of the space under the bridge.



COLUMNS

The columns under the bridge present a great opportunity to create neighborhood identity through artistic imagery.

AIRWAY BOULEVARD

✓PRIORITIZED

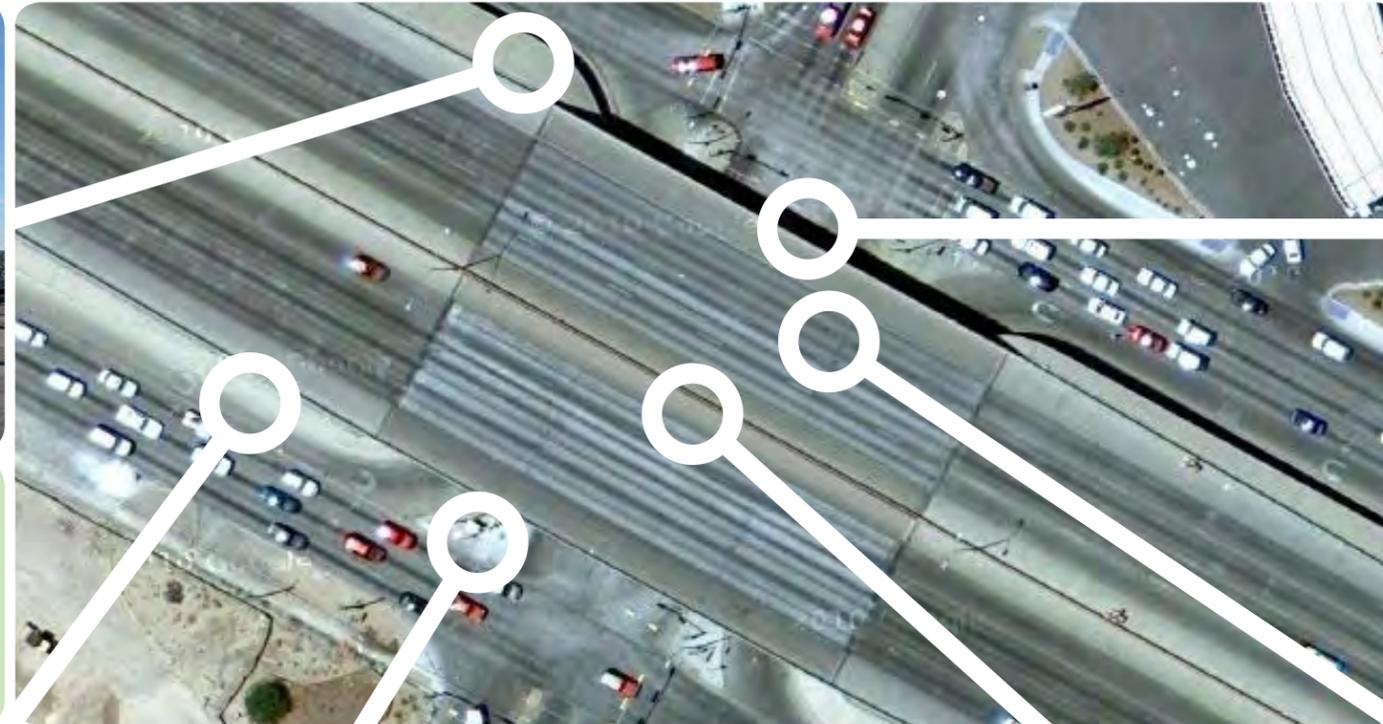
Airway establishes both the main link between the City and the Airport and a gateway for tourists and locals alike. Opportunities at Airway include the bridge span, paint, lighting, columns, slope paving, medians, pedestrian circulation and landscape. Creating artistic identity features on the span and columns, and lightening the bridge's underside with paint and LED lighting, mitigates and softens the harsh light conditions at the site.

Addressing slope paving, medians, pedestrian circulation, and landscape creates a cohesive concept across the entirety of the project. Adding elements of human scale; using sequential patterning and color to lighten the infrastructure; and softening the ground plane with landscaping and paving patterns promotes neighborhood identity while transforming the bridge into a true gateway experience.



LANDSCAPING

Swaths of landscaping cut into the slope paving create soft rhythms of greenery with native and hardy trees and shrubs.



BRIDGE SPAN FACADE

The facade of the bridge span offers a location to implement an artistic intervention to create a sense of place.



SLOPE PAVING

The slope paving presents a monotonous and barren landscape; covering it with rock creates a visually compelling ground plane.



MEDIANS

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UNDER THE BRIDGE

Both painting and light fixture choices lighten and improve the feeling of the space under the bridge.



COLUMNS

The columns under the bridge present a great opportunity to create neighborhood identity through artistic imagery.

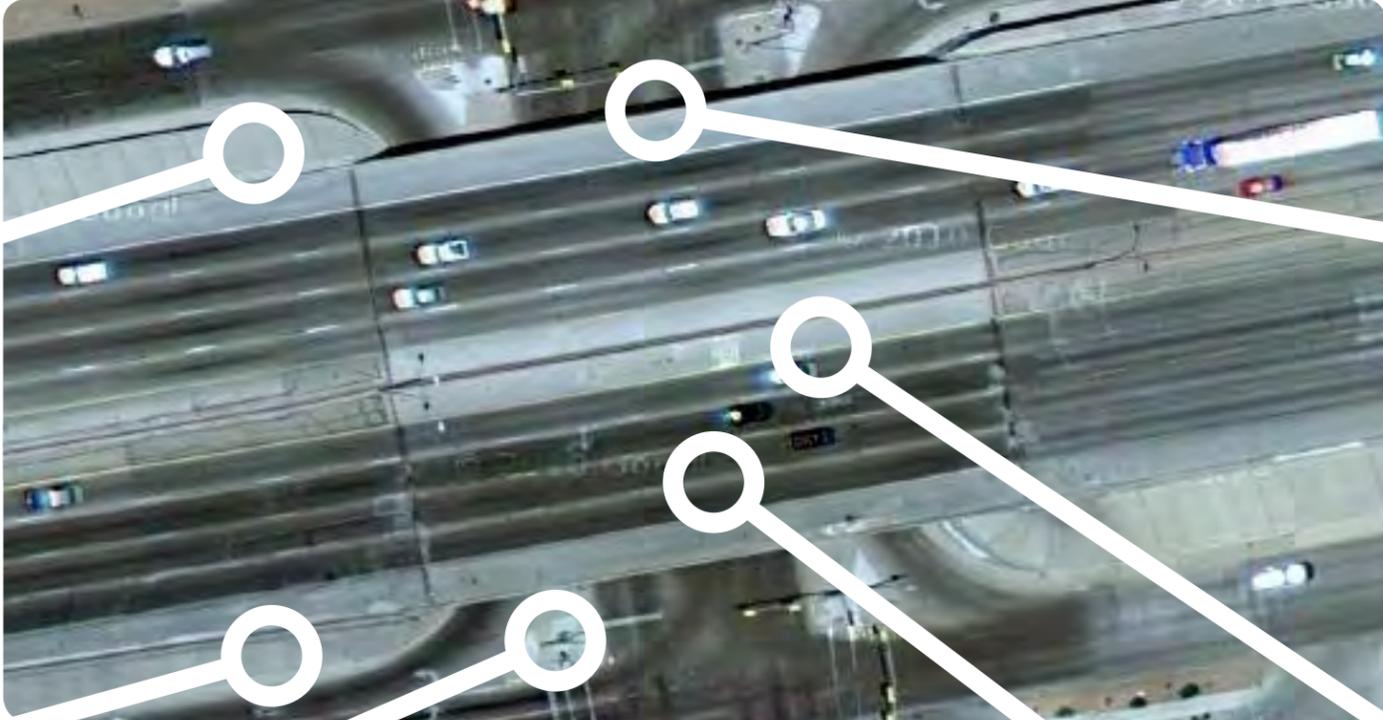
GERONIMO DRIVE

Geronimo unites a neighborhood with a mall, a church, and residences nearby; creating a community feel with many well-kept single family homes within a few blocks of the infrastructure. Wrought iron fencing distinguishes the residences. Nearby, a thriving mall houses popular retailers, including Costco, Marshalls, Kohls, Office Depot and more. Unfortunately the infrastructure feels run down and does not support its community role. This presents a great opportunity for improvement and connection.

Addressing Hawkins, Airway and Geronimo as a group reinforces the area as a retail destination. Opportunities at Geronimo include landscaping; promoting pedestrian circulation; sidewalk patterning and adding elements that provide human scale; creating artistic identity features on the span and columns; and lightening the bridge's underside with paint and LED lighting. These improvements give the area a more inviting feel in character with the rest of the project.



LANDSCAPING
Swaths of landscaping cut into the slope paving create soft rhythms of greenery with native and hardy trees and shrubs.



BRIDGE SPAN FACADE
The facade of the bridge span offers a location to implement an artistic intervention to create a sense of place.



SLOPE PAVING
The slope paving presents a monotonous and barren landscape; covering it with rock creates a visually compelling ground plane.



MEDIANS
The medians and edges of the site represent perfect locations for human scaled landscape features, making the area more welcoming.



UNDER THE BRIDGE
Both painting and light fixture choices lighten and improve the feeling of the space under the bridge.



COLUMNS
The columns under the bridge present a great opportunity to create neighborhood identity through artistic imagery.

TROWBRIDGE DRIVE

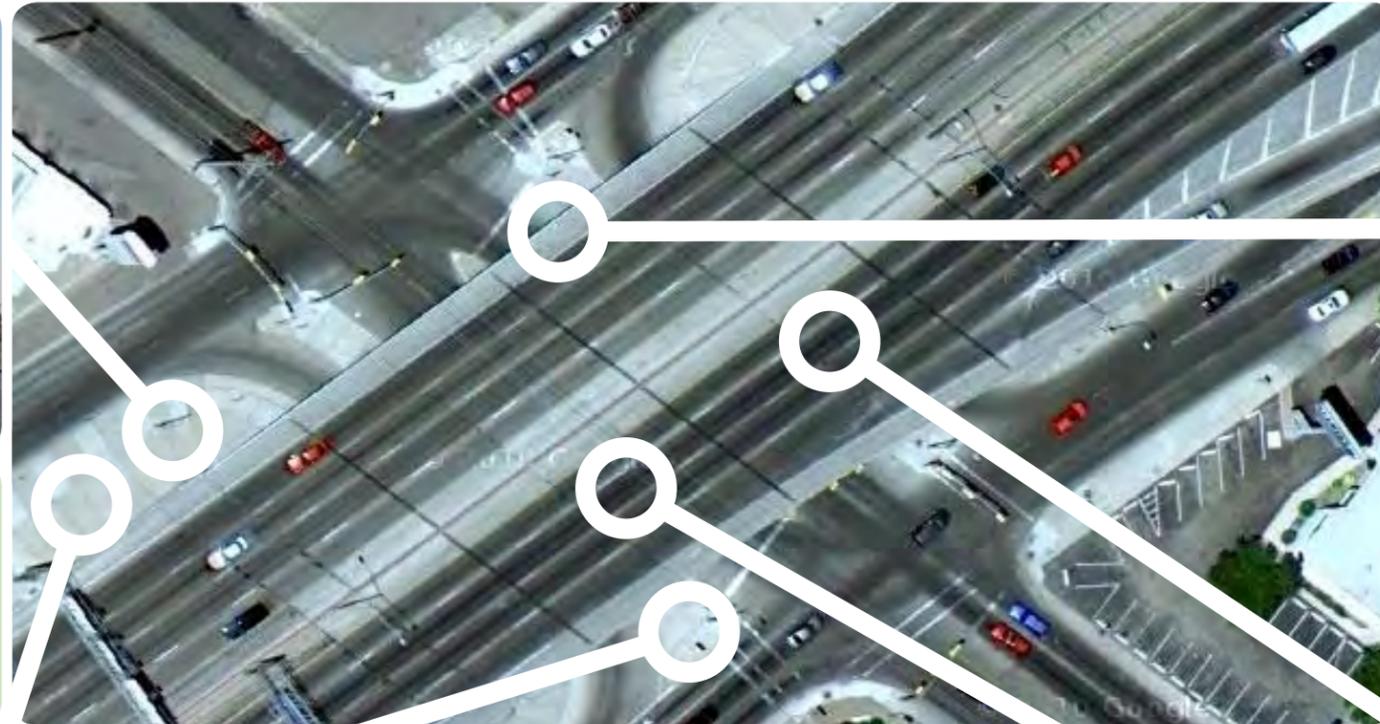
Trowbridge connects the town diagonally linking the residential neighborhoods on either side of I-10. Opportunities at Trowbridge include landscaping; promoting pedestrian circulation; sidewalk patterning and adding elements that provide human scale; creating artistic identity features on the span and columns; and lightening the bridge's underside with paint and LED lighting.

Trowbridge looms vast with many columns and many hard-scape surfaces in a commercial/industrial area that demands a unique solution to create a sense of identity. Incorporating human and site scaled aesthetic elements, in all the components of the infrastructure; and transforming the columns, spans, vertical walls, slope paving, pedestrian walkways, lighting, and nearby grounds lessens the massive nature of the infrastructure.



LANDSCAPING

Swaths of landscaping cut into the slope paving create soft rhythms of greenery with native and hardy trees and shrubs.



BRIDGE SPAN FACADE

The facade of the bridge span offers a location to implement an artistic intervention to create a sense of place.



SLOPE PAVING

The slope paving presents a monotonous and barren landscape; covering it with rock creates a visually compelling ground plane.



EDGES

The medians and edges of the site represent perfect locations for human scaled landscape features, making the area more welcoming.



UNDER THE BRIDGE

Both painting and light fixture choices lighten and improve the feeling of the space under the bridge.



COLUMNS

The columns under the bridge present a great opportunity to create neighborhood identity through artistic imagery.

PAISANO DRIVE

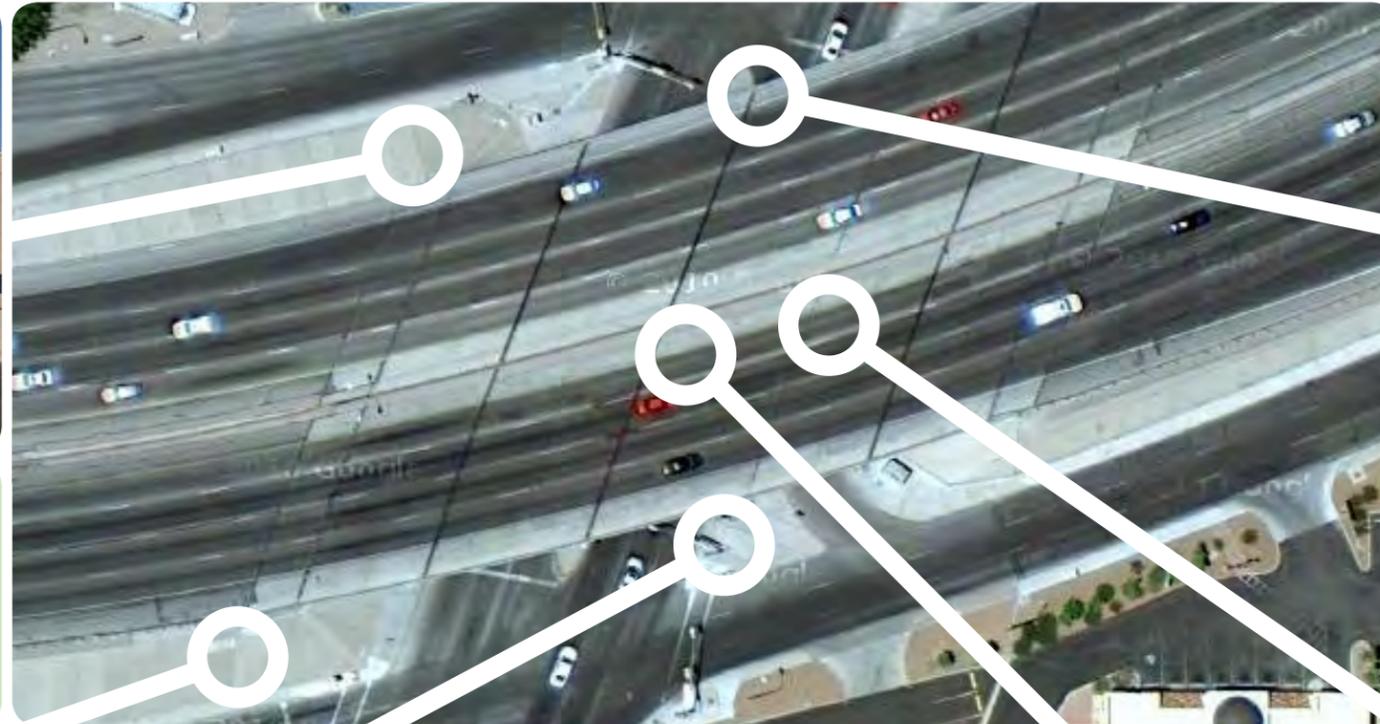
Paisano connects north and south as an important through street in the area. Its large scale dominates the neighborhoods around it. Incorporating aesthetic improvements in concert with the whole concept scheme fits the bridge into its context better.

Opportunities at Paisano include landscaping; promoting pedestrian circulation; sidewalk patterning and adding elements that provide human scale; creating artistic identity features on the span and columns; and lightening the bridge's underside with paint and LED lighting. These changes greatly improve the site making the bridge a highlight on the path of Paisano as it cuts through town.



LANDSCAPING

Swaths of landscaping cut into the slope paving create soft rhythms of greenery with native and hardy trees and shrubs.



BRIDGE SPAN FACADE

The facade of the bridge span offers a location to implement an artistic intervention to create a sense of place.



SLOPE PAVING

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UNDER THE BRIDGE

Both painting and light fixture choices lighten and improve the feeling of the space under the bridge.



COLUMNS

The columns under the bridge present a great opportunity to create neighborhood identity through artistic imagery.

CHELSEA STREET

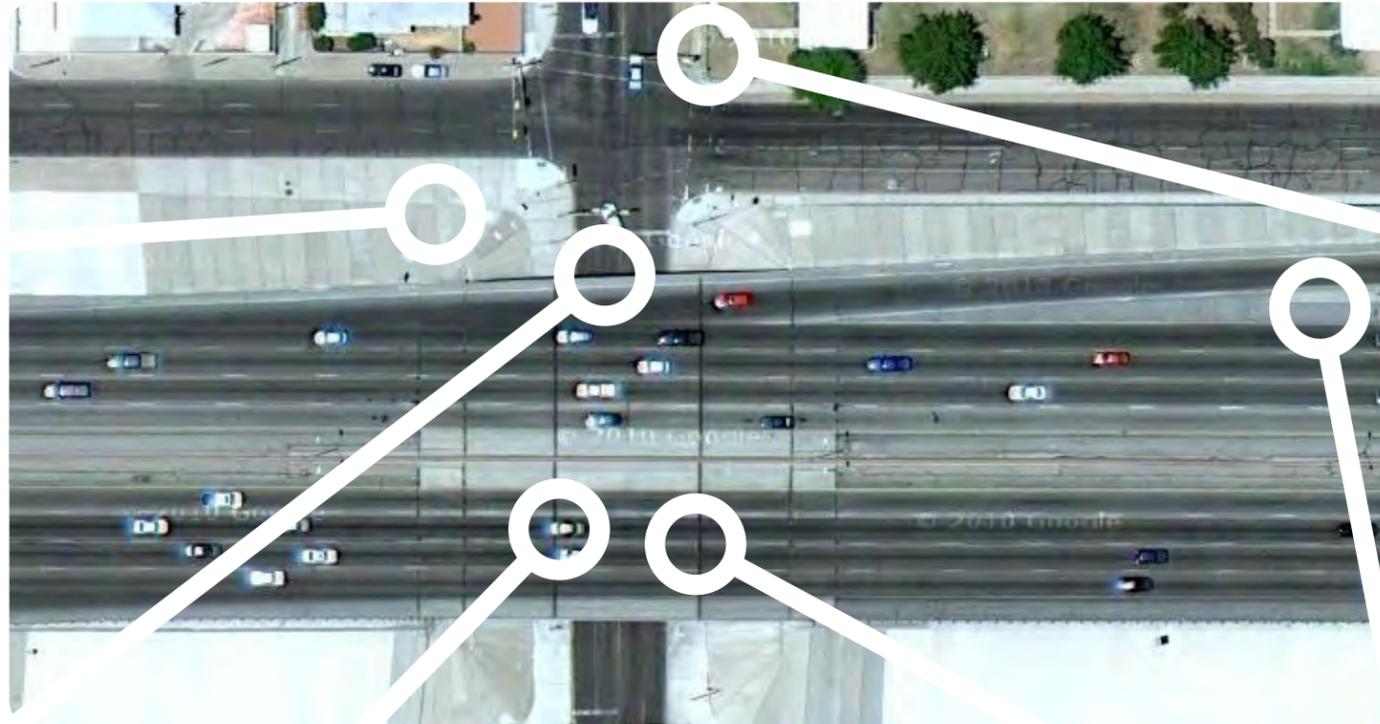
Chelsea displays a strong neighborhood character interrupted by the tall steep slope paving of I-10 and the dark underside of its bridge. The slope paving meets the ground without the mediation of an MSE wall and has an unfinished feel. The neighborhood around the bridge transitions from the commercial/industrial area of the east to a more residential community to the west.

Pulling elements from the neighboring communities into the infrastructure gives the bridge a stronger connection to place and a more complete design. Opportunities at Chelsea include landscaping; promoting pedestrian circulation; sidewalk patterning and adding elements that provide human scale; creating artistic identity features on the span and columns; and lightening the bridge's underside with paint and LED lighting.



LANDSCAPING

Swaths of landscaping cut into the slope paving create soft rhythms of greenery with native and hardy trees and shrubs.



STREETSCAPE

The existing streetscape elements provide a reference for bringing the neighborhood feel into the infrastructure of the bridge.



BRIDGE SPAN FACADE

The facade of the bridge span offers a location to implement an artistic intervention to create a sense of place.



COLUMNS

The columns under the bridge present a great opportunity to create neighborhood identity through artistic imagery.



UNDER THE BRIDGE

Both painting and light fixture choices lighten and improve the feeling of the space under the bridge.



SLOPE PAVING

The slope paving presents a monotonous and barren landscape; covering it with rock creates a visually compelling ground plane.

COPIA STREET

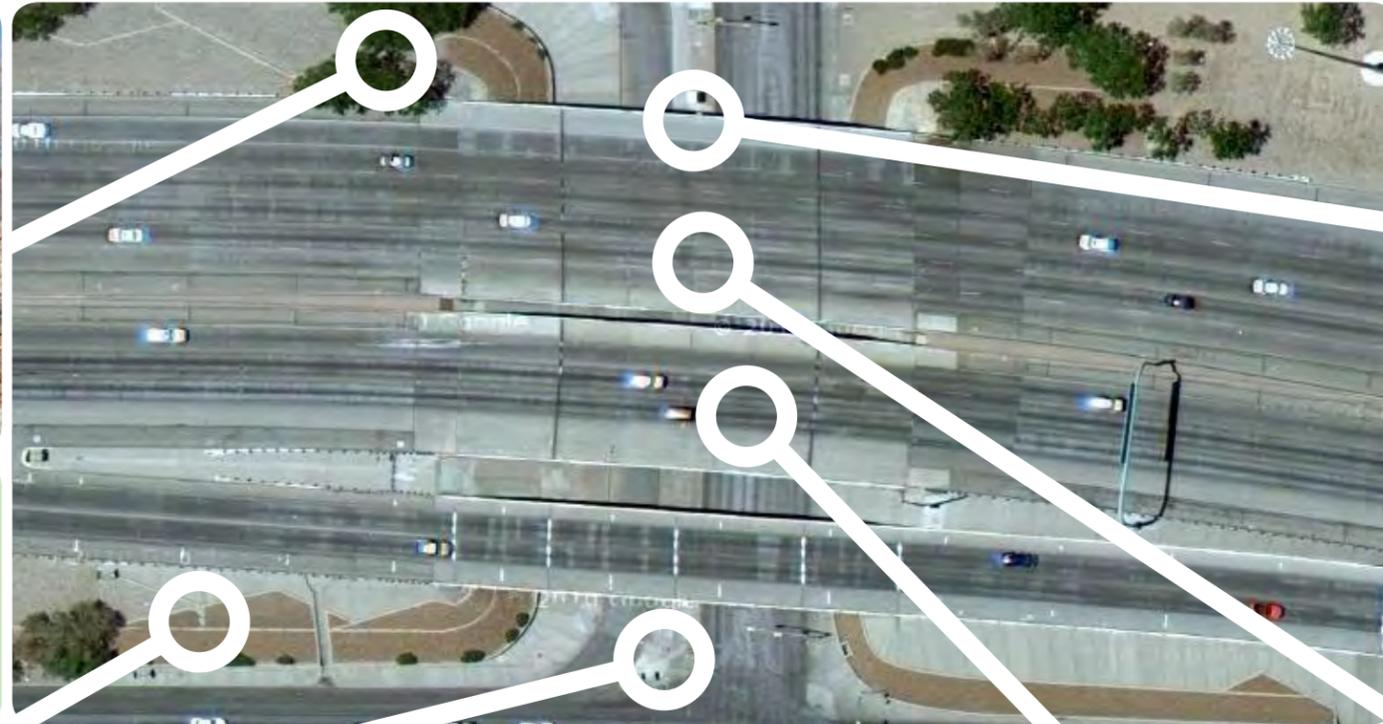
Copia serves as a successful precedent for managing large amounts of slope paving. The use of colored rock aggregate and landscaping helps to create a more pleasant experience. However, this example should not merely be copied and much more can be done to improve the bridge.

Opportunities at Copia include using the span and columns to create artistic identity features and using paint and lighting to lighten the underside of the bridge. The improvements to Copia must not only address the specific needs of its site but also fall within the larger scheme, especially the transitions between existing and proposed slope paving treatments.



LANDSCAPING

The many trees and plants clustered together help to create a sense of rhythm that softens the experience of being on or near I-10.



BRIDGE SPAN FACADE

The facade of the bridge span offers a location to implement an artistic intervention to create a sense of place.



SLOPE PAVING

Covering the slope paving with rock creates a visually compelling ground plane. Designs and patterns add neighborhood identity.



MEDIANS

The medians and edges of the site represent perfect locations for human scaled landscape features, making the area more welcoming.



UNDER THE BRIDGE

Both painting and light fixture choices lighten and improve the feeling of the space under the bridge.



COLUMNS

The columns under the bridge present a great opportunity to create neighborhood identity through artistic imagery.

PIEDRAS STREET

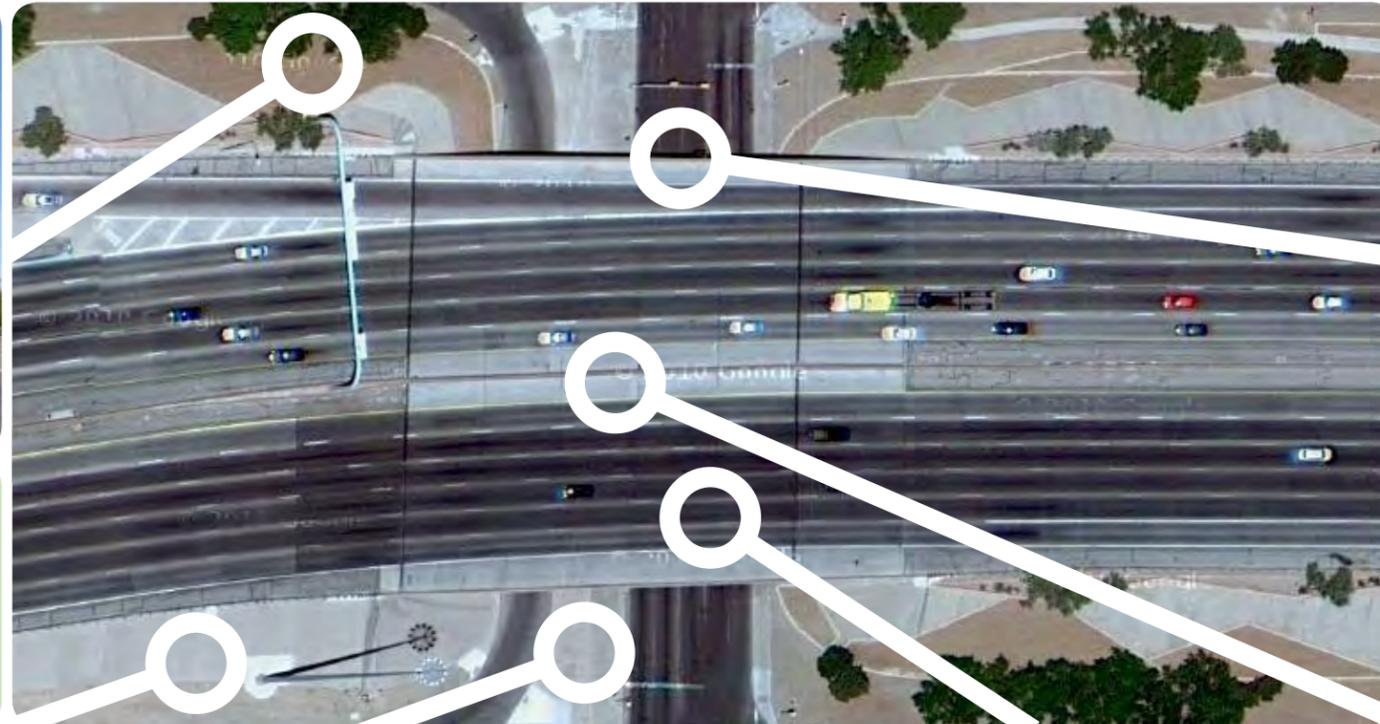
Piedras serves as a successful precedent for managing large amounts of slope paving. The use of colored rock aggregate and landscaping helps to create a more pleasant experience. However, this example should not merely be copied and much more can be done to improve the bridge.

Opportunities at Piedras include using the span and columns to create artistic identity features and using paint and lighting to lighten the underside of the bridge. The improvements to Piedras must address the specific needs of its site while also managing the transitions between existing and proposed slope paving treatments.



LANDSCAPING

The many trees and plants clustered together help to create a sense of rhythm that softens the experience of being on or near I-10.



BRIDGE SPAN FACADE

The facade of the bridge span offers a location to implement an artistic intervention to create a sense of place.



SLOPE PAVING

Covering the slope paving with rock creates a visually compelling ground plane. Designs and patterns add neighborhood identity.



MEDIANS

The medians and edges of the site represent perfect locations for human scaled landscape features, making the area more welcoming.



UNDER THE BRIDGE

Both painting and light fixture choices lighten and improve the feeling of the space under the bridge.



COLUMNS

The columns under the bridge present a great opportunity to create neighborhood identity through artistic imagery.

COTTON STREET

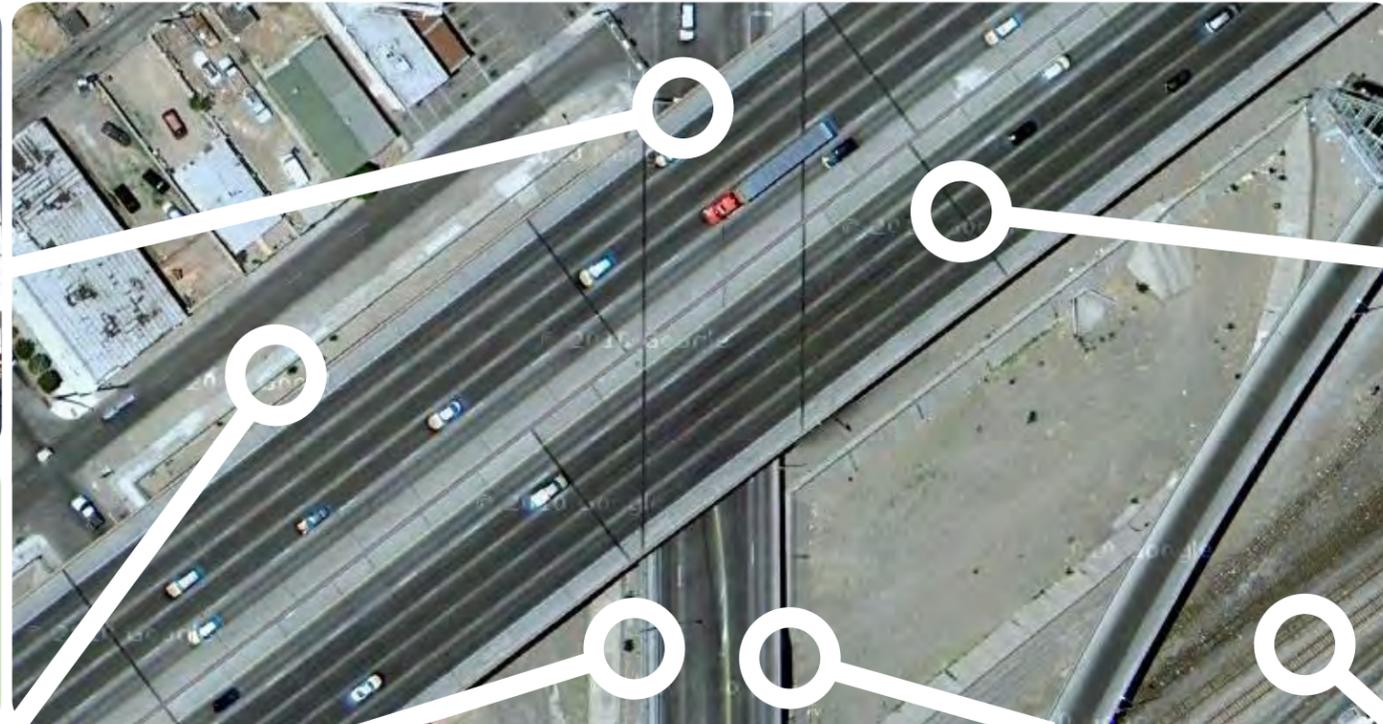
Cotton represents a unique situation along the I-10 corridor. The elevated highway sits on very tall columns as it runs alongside the railroad tracks. Cotton itself runs under the highway and then bridges over the railroad tracks. Aesthetic elements added to this bridge portion of Cotton tie it back to its railroad context.

These elements include train imagery in some railings and other geometric patterns elsewhere. They need specific attention to determine how to improve these existing features. If in the future the train yard moves, the underside of the highway and its columns become valuable place making elements.



BRIDGE SPAN FACADE

The facade of the bridge span offers another location to implement an artistic intervention to create a sense of place.



COLUMN FIELD

The column field under the highway offers a large opportunity for artistic intervention to add interest to an otherwise dead space.



EDGES

Planting in the medians and edges of the underside of the bridge masks its height and dominating appearance.



GRAPHIC RAILING

The existing railing with the train graphics necessitates review to incorporate it into the total design scheme.



LANDSCAPING

Landscaping will improve the barren nature of the drainage ditch transforming it into a verdant bio-retention swale.



RAIL YARD

If the rail yard becomes vacant, its future form must include possible community uses incorporated into the total design scheme.

RAILROAD ARCHES

✓PRIORITIZED

The arches along the south side of 1-10 bordering the rail yard stand as a defining icon in this area. They welcome those traveling west into the downtown and follow those exiting downtown from East Franklin. The fifteen groups of three arches march along the railroad and reflect the arches of the Old Train Depot. A decorative gate marks each end of the array of arches. When they were installed, more than fifteen years ago, a system of fiber optics created a light show that animated the arches at night.

Now the arches stand in disrepair. The lights no longer function. The steel structures are rusty. Still the idea of the arches presents a great opportunity to create identity along the highway. If rebuilt they will once again make an evocative statement. The arches, railings and their supports must be removed and reconstructed with modern lighting technology. Other improvements include landscaping the medians and along the fence.



GATES

The gates at the beginning and end of the arch sequence use forms reminiscent of El Paso's wrought iron metalwork.



ARCHES FRONT

The roadside arches once housed a dramatic lighting feature that sequenced and changed colors. Many locals recall this feature fondly.



MEDIANS

The medians and edges of the site represent opportunities for vehicular scaled landscape features, making a welcoming gesture.



OLD TRAIN DEPOT

The Old Train Depot displays a striking array of arches that serve as a reference for the metal decorative arches along the roadway.



ARCHES BACK

The backside of the arches face an empty lot that would benefit from landscaping.



ARCH DETAIL

The rusty arches and railings need replacing which requires the complete rebuilding of the brick supports.

DOWNTOWN BRIDGES

✓PRIORITIZED

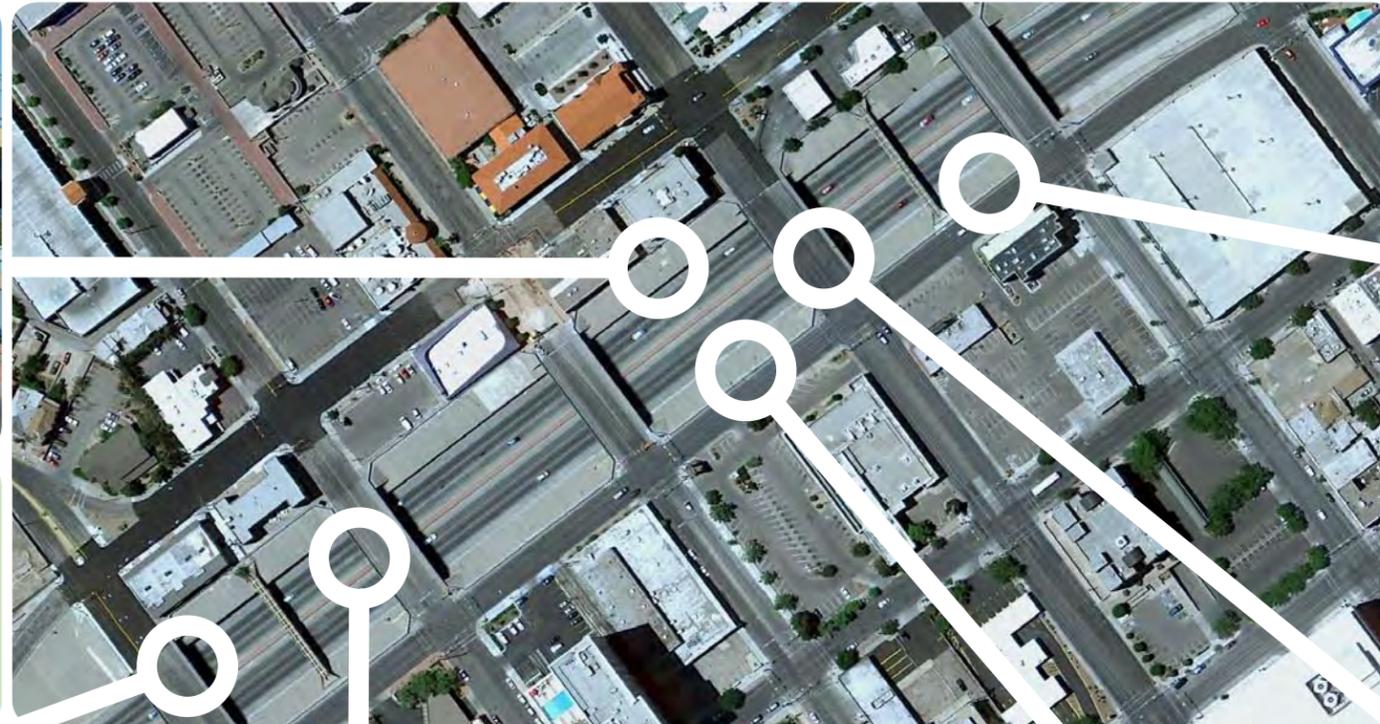
Downtown is a unique situation along the I-10 corridor. The highway is depressed below the grade of the street grid resulting in many bridge crossings over I-10. Previous aesthetic efforts made to this area of highway include painting the bridges many colors and painting murals on the walls.

Whatever design measures are taken in the downtown, they must serve to unify it with the greater design scheme while also highlighting its importance as the center of the urban experience. Rethinking color, railings, lighting, and slope paving is key to success downtown.



MURALS

The murals on the roadway walls are figurative symbols in bold colors that need to be updated for a contemporary urban space.



SLOPE PAVING

The slope paving presents a monotonous and barren landscape; covering it with rock creates a visually compelling ground plane.



BRIDGE SPAN FACADES

The facades of the bridge spans are painted in many light pastel colors and need to be refreshed to relate to the murals.



LIGHTING

Lighting along the roadway presents an opportunity to create dramatic identity elements that add life to the bridges.



LANDSCAPING

Swaths of landscaping cut into the slope paving create soft rhythms of greenery with native and hardy trees and shrubs.

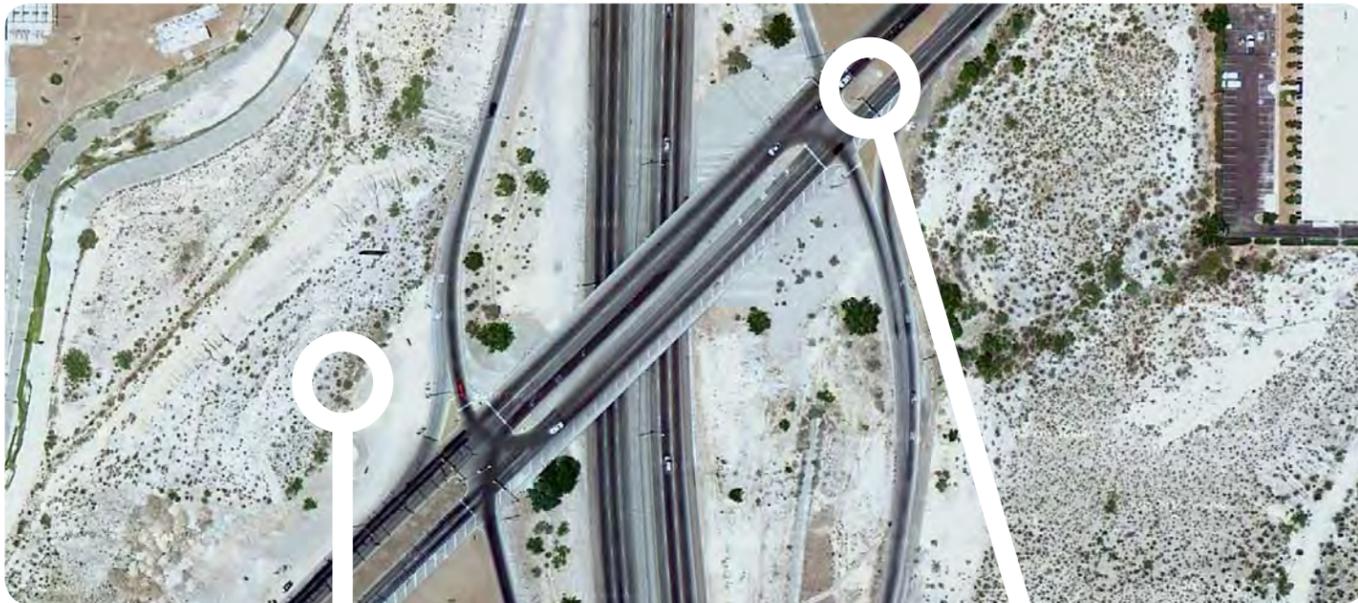


RAILINGS

The railings of the bridges can be improved to create a sense of place, designing them to relate to context will tie them to the city.

EXECUTIVE CENTER BOULEVARD

Executive represents a unique situation along the I-10 corridor. The surrounding landscape grows in a natural manner creating a powerful sense of place. The native plants and trees soften the hard edges of the infrastructure and blend it into the landscape. However, the medians feel too manicured and contradict the adjacent landscape. Approaching the medians with the same concept as the nearby landscape reinforces the total scheme. As the farthest west bridge on the project scope, Executive serves as an introduction to the city.



LANDSCAPING

The natural landscape presents a successful precedent for the I-10 corridor, using trees and native plants to soften the infrastructure.



MEDIANS

These medians offer a precedent for stone, but the plants are too controlled. A natural expression ties the medians to the landscape.

SITE REVIEW SUMMARY

The bridges share more in common across the scope of the I-10 corridor than they differ. These similarities facilitate the application of a cohesive scheme for aesthetic improvements. Modifying their slope paving, landscaping and infrastructure in a range of treatments unifies the total experience.

The bridges to the east are all highway bridges over neighborhood streets. It makes sense to treat these locations similarly within a larger plan.

The bridges at the rail road yard, downtown and to the west represent unique situations and must be considered on their own while fitting into the total corridor plan.

Positioned into the cohesive aesthetic scheme, each bridge must also

appear as a distinct element within the larger plan. Because the bridges read as indistinguishable from each other, they lack a sense of place. Any aesthetic applications must create a specific identity for each bridge reflecting its neighborhood.

The bridge facades, columns and lighting schemes are key opportunities to make these specific identity statements. Each bridge must make its own statement, but they all must also speak the same aesthetic language.

Many of the proposed solutions already exist in and around El Paso as demonstrated in the precedents illustrated in Part 2. Following and drawing influence from these successful examples helps to tie the scheme back into the local context.

PART 2: DIAGRAMMING

To be successful, a cohesive scheme applied across the whole corridor must pull together many elements while allowing for unique expressions. Carefully considering each of the elements maximizes their potential within the scheme. By compiling and analyzing precedent examples, a sense of what others have done can be gained. This knowledge of previous work influences and inspires the direction of future planning. These ideas for designs then get diagrammed out to understand the possible successful variations. The diagrams allow for a general understanding of design recommendations and potential implementation before beginning design. The recommended improvements are illustrated in composite photo collages to aid in site visualization.

Each bridge, named in columns across the top, presents opportunities for aesthetic improvements, named in rows down the side. The boxes filled with color represent a specific opportunity existing at a specific bridge. Later, the book illustrates each element of aesthetic improvement. Refer to the following pages for guidance in choosing and applying these improvements to the bridges.

AESTHETIC IMPROVEMENTS

| | FIRST PRIORITY | | | 10 to 20 Year Plan Ranked by Priority | | | | | | | | |
|-------------------------------|-------------------|-----------------------|-----------------------|---------------------------------------|----------------------|---------------------|-----------------------|--------------------|--------------------|------------------|--------------------|------------------|
| | Airway \$ 10 M | Downtown \$ 44.5 M | RR Arches \$ 1.8 M | Hawkins \$ 10 M | Executive \$ 12 M | Geronimo \$ 10 M | Trowbridge \$ 20 M | Paisano \$ 15 M | Chelsea \$ 10 M | Copia \$ 20 M | Piedras \$ 10 M | Cotton \$ 8 M |
| Slope Paving | Color | Color | Color | Color | Color | Color | Color | Color | Color | Color | Color | Color |
| Landscaping Including Walls | Color | Color | Color | Color | Color | Color | Color | Color | Color | Color | Color | Color |
| Medians | Color | | | Color | Color | Color | Color | Color | Color | Color | Color | Color |
| Neighborhood Identity | Color | Color | Color | Color | Color | Color | Color | Color | Color | Color | Color | Color |
| Facades | Color | Color | | Color | | Color | Color | Color | Color | Color | Color | Color |
| Columns | Color | Color | | Color | | Color | Color | Color | Color | Color | Color | Color |
| Lighting | Color | Color | Color | Color | | Color | Color | Color | Color | Color | Color | Color |
| Walkways | Color | Color | | Color | Color | Color | Color | Color | Color | Color | Color | Color |
| Abutments | Color | | | Color | | Color | Color | Color | Color | Color | Color | |
| Walls (MSE and Cast in Place) | Color | Color | | Color | Color | Color | Color | Color | Color | Color | Color | Color |
| Railings | | Color | Color | | | | | | | | | Color |

SLOPE PAVING



The rock aggregate at Copia serves as a good precedent example for informing future slope paving design solutions.



The many colors and textures of rock aggregate at Piedras represent a good precedent for future design solutions.



Colored rock aggregate creates a texturally dynamic surface that softens the slope paving making it pleasant looking.



The field of stone relates the ground plane back to the local natural geology enhancing the sense of place.



Another solution involves using gabion wire mattresses filled with local rocks and placed directly on top of the slope paving.



Gabions require less maintenance than rock aggregate and have easier access once built; giving a clean controlled look.

PRECEDENTS

The ubiquitous slope paving represents the largest area for aesthetic enhancement along I-10. Adding local rock on top of the slope paving or painting it creates color and textural interest.



Rock aggregate in many colors placed on the slope in a shape or motif adds visual interest to the sides of the roads.



The contrast between the colors can vary creating different visual expressions; high contrast creates a more graphic look.



Phoenix's Black Canyon freeway uses slope paving much like that on the I-10 corridor creating a barren un-welcoming experience.



However, removing and replacing it with landscaping and rock aggregate creates a better, more inviting sense of place.

LANDSCAPING



Trees create a sense of verticality that in concert with shrubs, walls and rocks, add a dramatic improvement to a site.



Even spread across a field of rock aggregate, trees, large rocks and shrubs give a powerful effect.



Dramatic lines of landscaping add powerful gestures to the site that requires a specific focus.

PRECEDENTS

Local and hardy trees and plants create a sustainable landscape along the I-10 corridor once the slope paving gets cut to allow for the planting of landscaping.



Large rocks and boulders add a geologic weight to a site and work to anchor the landscaping.



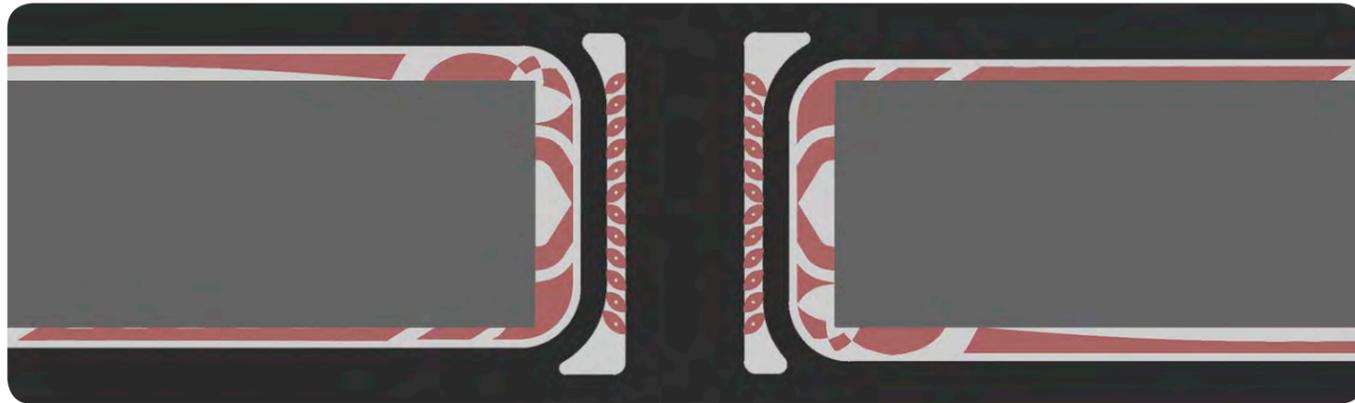
El Paso has a beautiful array of native plants and a natural approach to landscaping creates a connection to place.



Clustering similar landscape elements together creates masses of plants that help to define space.

SLOPE PAVING

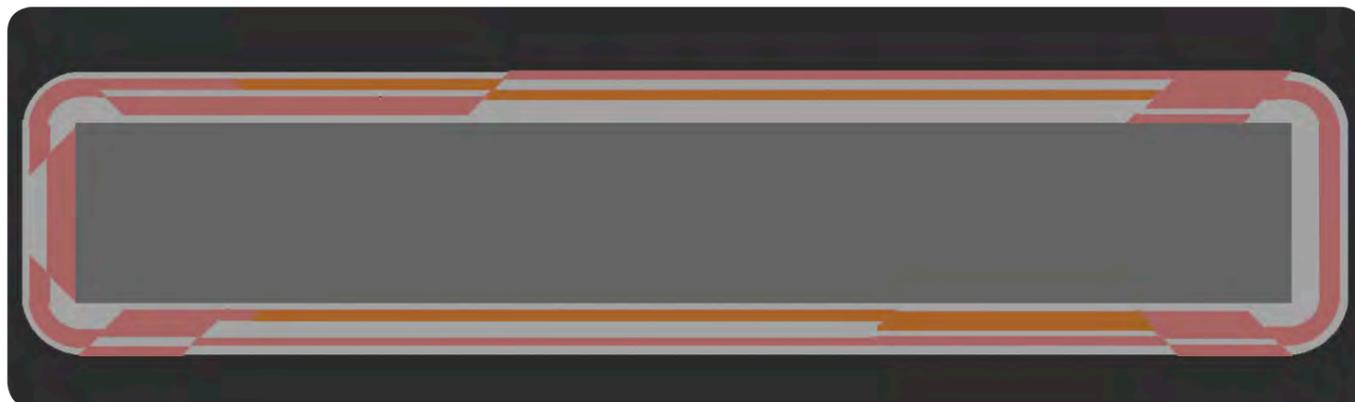
VARIATIONS



A larger pattern made from rock aggregate, based on radial geometry and local influences, creates a sense of rhythm in the ground plane.



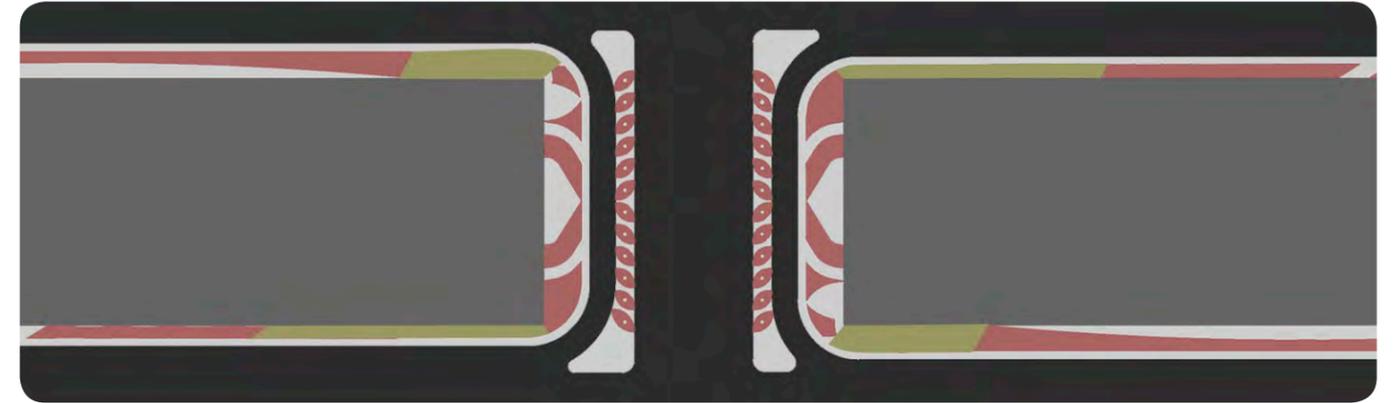
A more intricate pattern of painted slopes adds a level of sophistication to the design and focuses attention to the intersection.



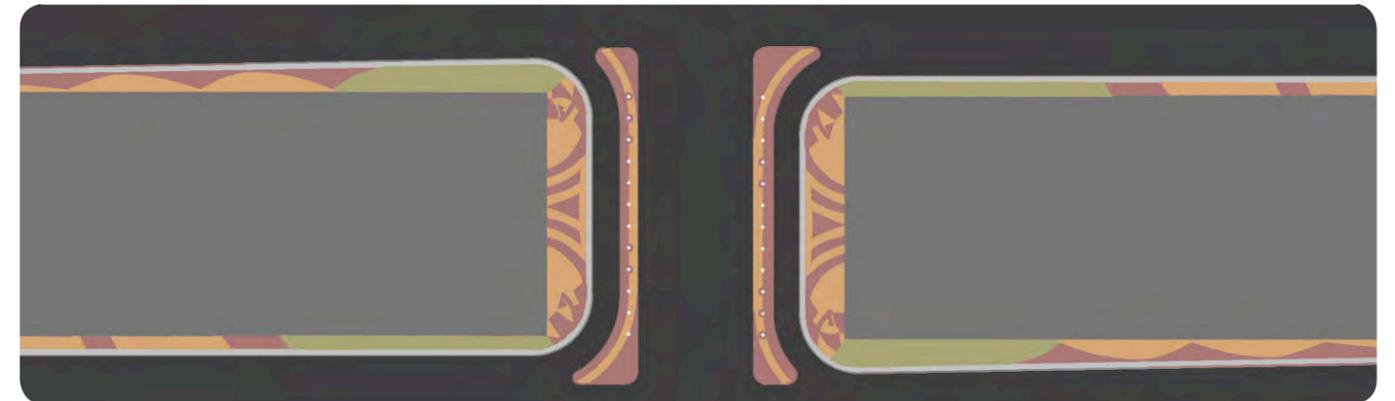
A scheme of long horizontal banding has a powerful geologic reference to the layers of rock in the surrounding mountains.

LANDSCAPING

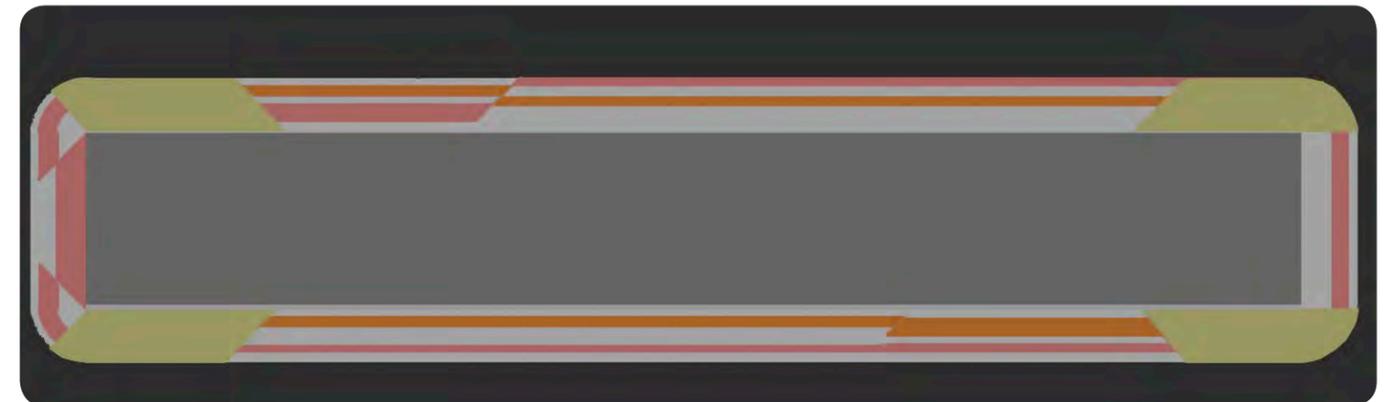
VARIATIONS



Putting in large swaths of landscaping into the slope paving introduces a natural element to the pattern, as shown in green.



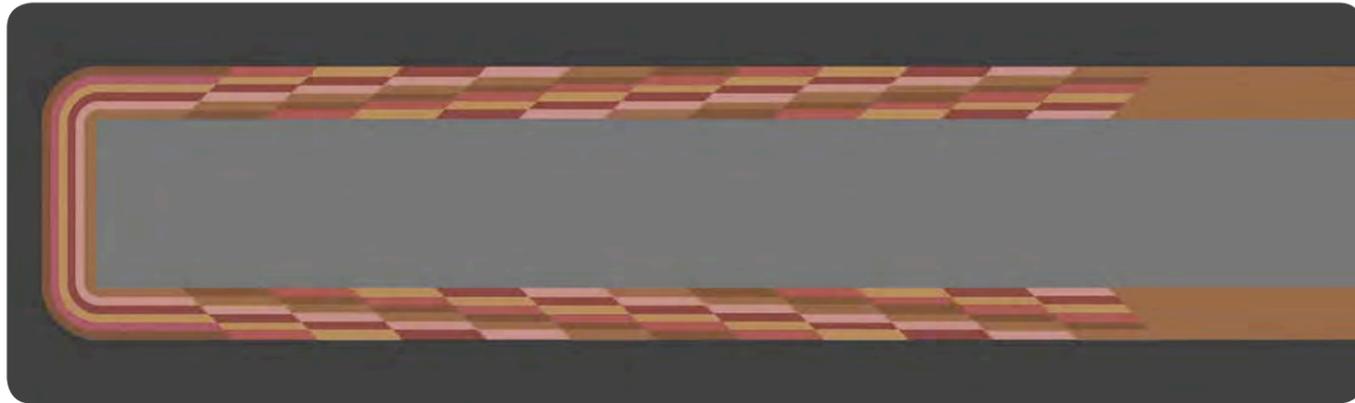
The strategic placement of landscaping near the bridges makes them more visible and expressive to those on the highway and around it.



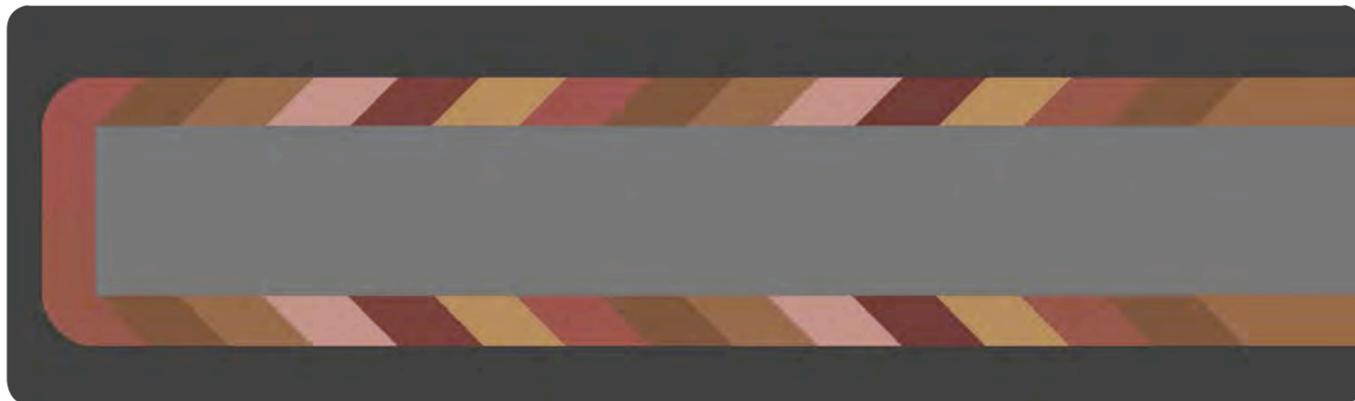
Landscape elements both soften the hard lines of the patterning and extend the design into the vertical space.

SLOPE PAVING

VARIATIONS



Patterns in paint on the existing slope paving easily create a large impact.



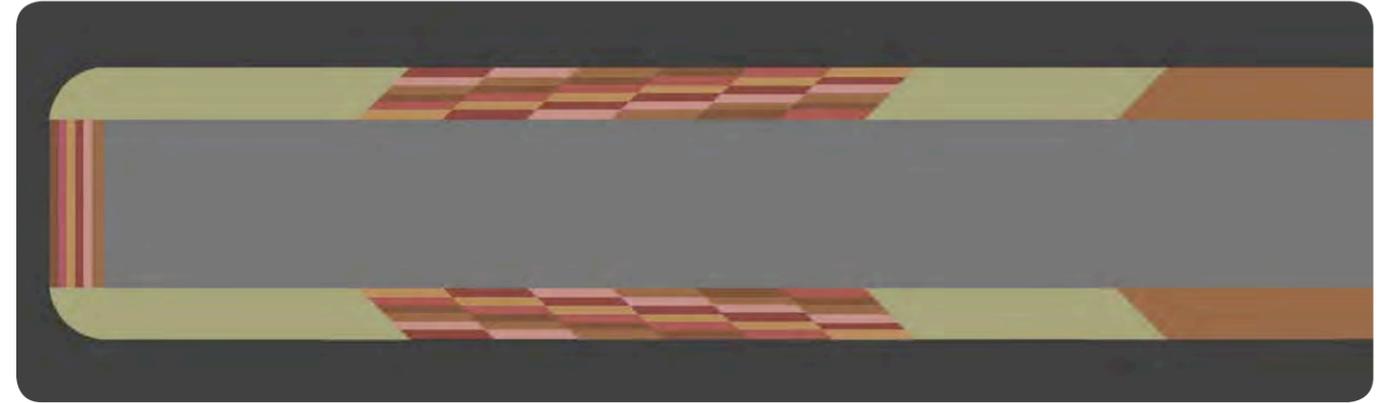
These painted patterns range in detail and complexity depending on the nature of the neighborhood they run through.



Combining sandblasting and painting expresses complex pattern work.

LANDSCAPING

VARIATIONS



Landscape at key areas, as shown in green, augments these painted patterns.



The landscaping serves to highlight the intersections and helps mediate the long expanses of slope paving in between them.



Putting taller more full landscape at the intersection makes them focal areas, whereas lower plants and ground cover fill the areas in the middle stretches.

MEDIANS



Simple gestures create powerful effects as seen here by simply contrasting the size and color of rock aggregate.



Using both trees and shrubs in a median creates two levels of interest that, even in winter, fill the space.



Clustering evergreen and deciduous trees in alternating groups creates patterning along the length of the median, pulling the eye along.

PRECEDENTS

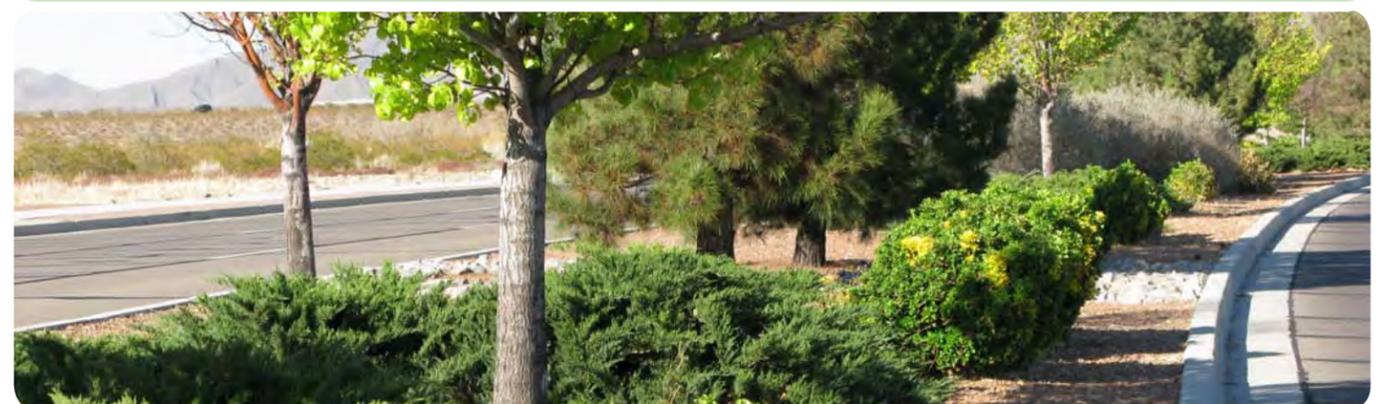
The medians adjacent to the bridges and their intersections require repair and pose an opportunity for landscape elements to improve the experience for those driving by them.



Medians full of landscaping at different levels, tall trees, low natives and bold large stones create visual interest.



Variety in the plant selection and in the rocks on the ground plane creates a dynamic textural interest broken up by large rocks and feature walls.

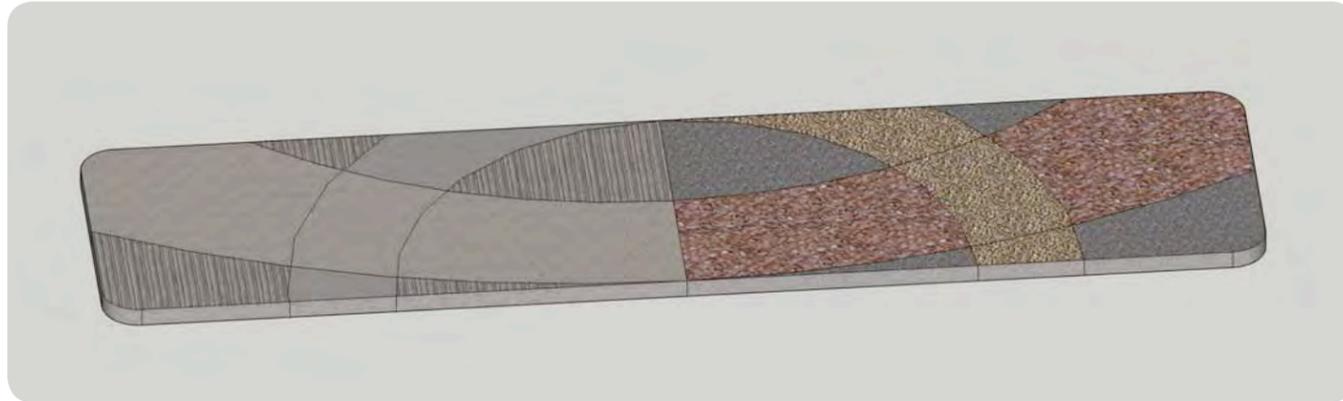


Density of layers in the medians creates a full visual and textural palette.

MEDIANS

VARIATIONS

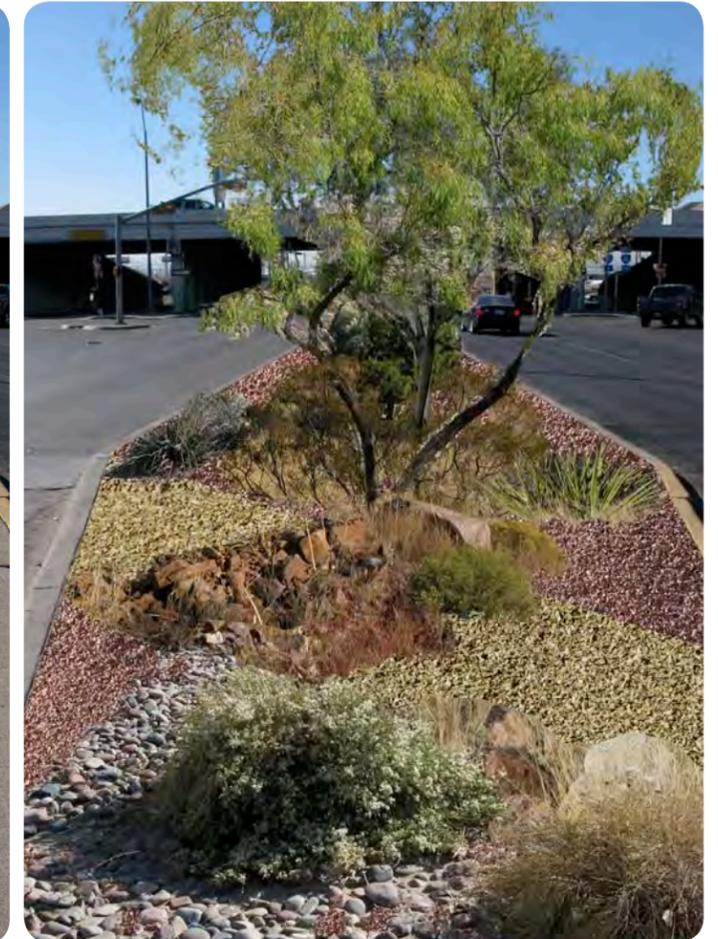
COMPOSITE



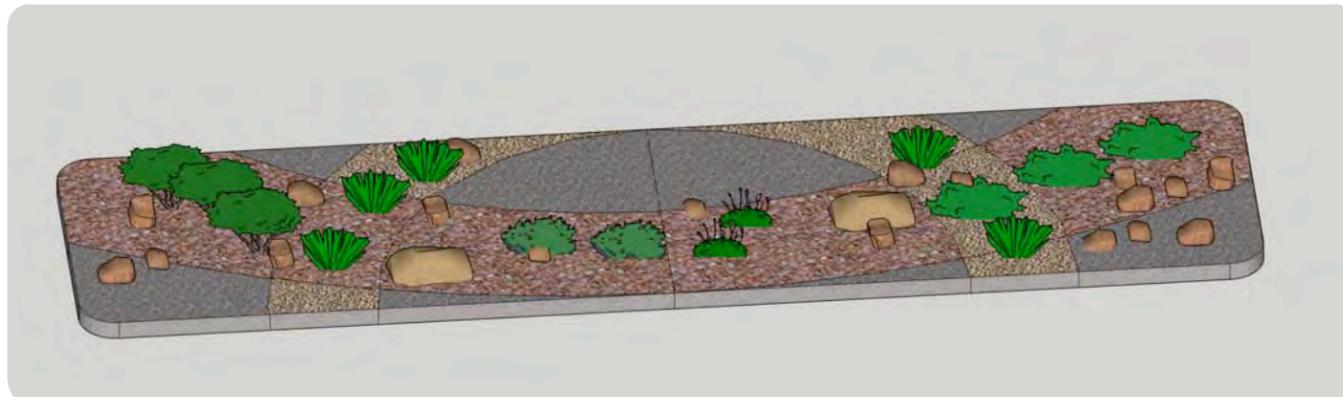
Where the medians must remain flat the best choice becomes either replacing the worn out cracked concrete with new stamped concrete or with colored rock aggregate.



The old worn median adds nothing aesthetically to the experience remaining flat and uninviting.



A full and natural landscape scheme creates visual interest and improves the roadway.



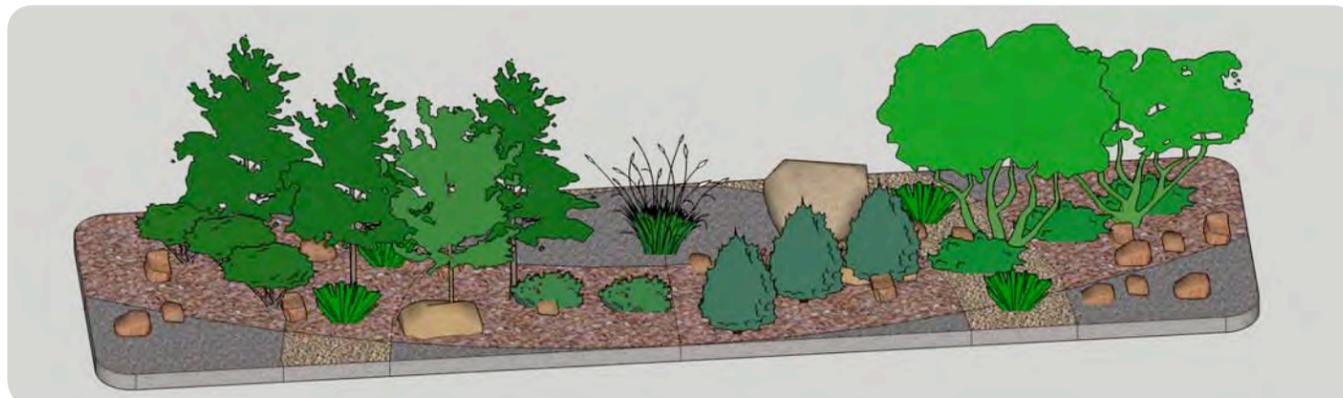
If possible, landscaping dramatically improves the median. Even an assortment of low plants helps to soften the surface and scattered large stones add contrast.



El Paso's natural landscape offers an abundant source for plant compositions.



The effective variety of textures in the ground plane and in the plant palette already exists in El Paso.



Adding landscaping at a variety of scales, colors, and textures, to gain visual interest and create space with rocks, shrubs, stones, and trees represents the best way to improve the medians.

IDENTITY

PRECEDENTS



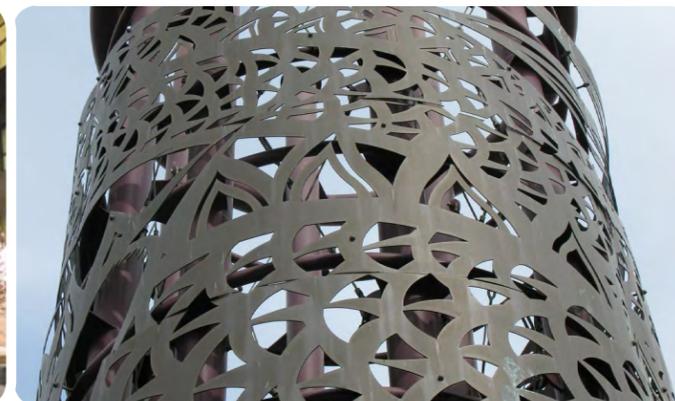
Murals showing the history of an area work together with landscaping and sculpture to enhance the area.



Colorful cut metal banners serve as way finding devices and as a means of introducing pattern into the site.



Sculptures, especially those that use light, create a sense of space.



Incorporating local patterns into an artwork offers another way to represent identity.



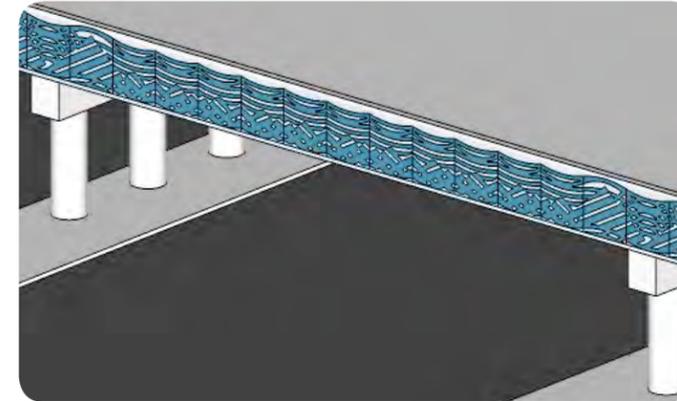
Not all murals stay on walls, here slope paving provides a location for local patterns and motifs.



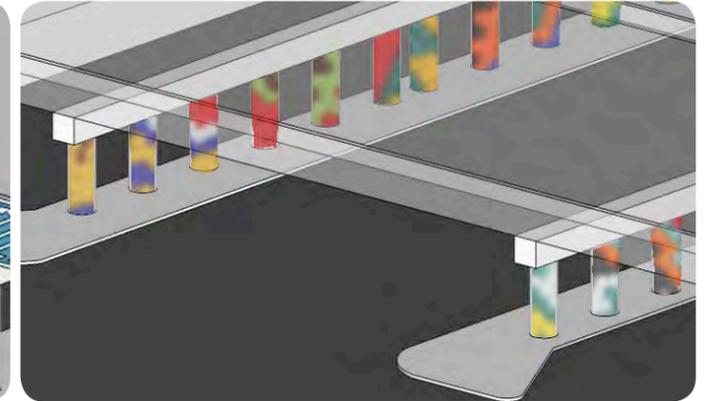
Introducing patterns into small details at a pedestrian scale creates a cohesive sense of place.

Each bridge runs through a unique neighborhood. Understanding these specific identities and then applying them to each bridge with artistic elements ties the bridges back to a sense of place.

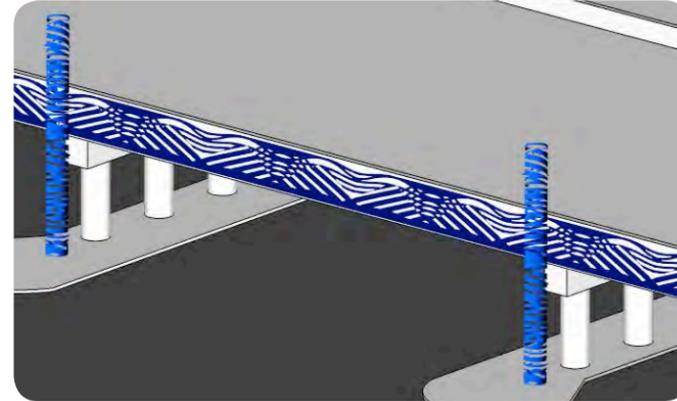
ELEMENTS



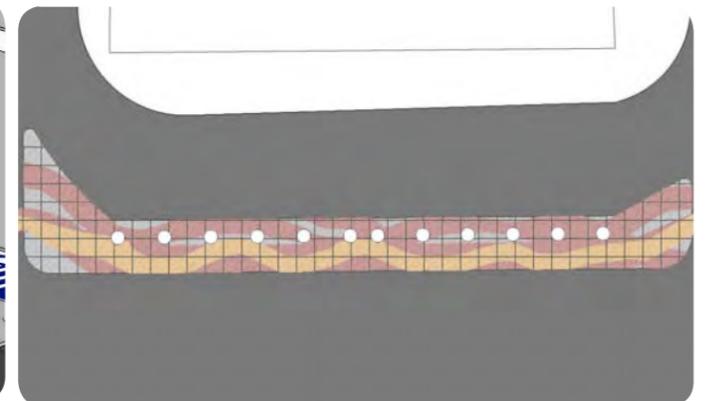
The bridge span facades present a useful location to create neighborhood identity.



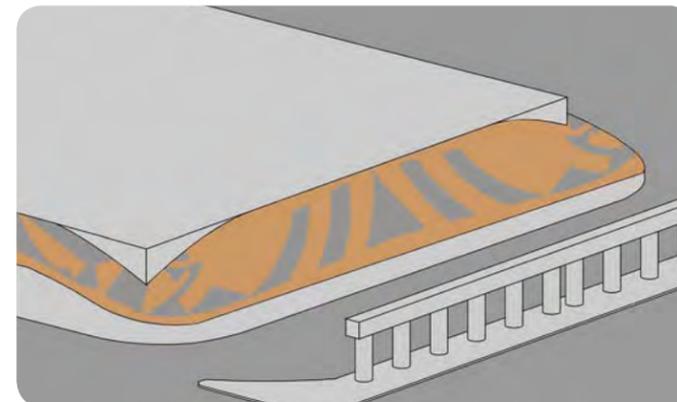
The columns under the bridge offer a great location for pedestrian scaled identity features.



Patterned screen light towers at the bridges signal their locations to a wide radius.



The pedestrian walkways require enhancements to better define the path and create a sense of safety and motion.



The abutments under the bridge serve as location for strong identity elements.



The railings on the downtown bridges must begin to take on the character of the neighborhoods they link.

COLUMNS



A field of columns painted many colors defines the space under this bridge.



El Paso has a precedent of painting columns. However, the light colors get washed out by the sun and lose intensity in shadow.



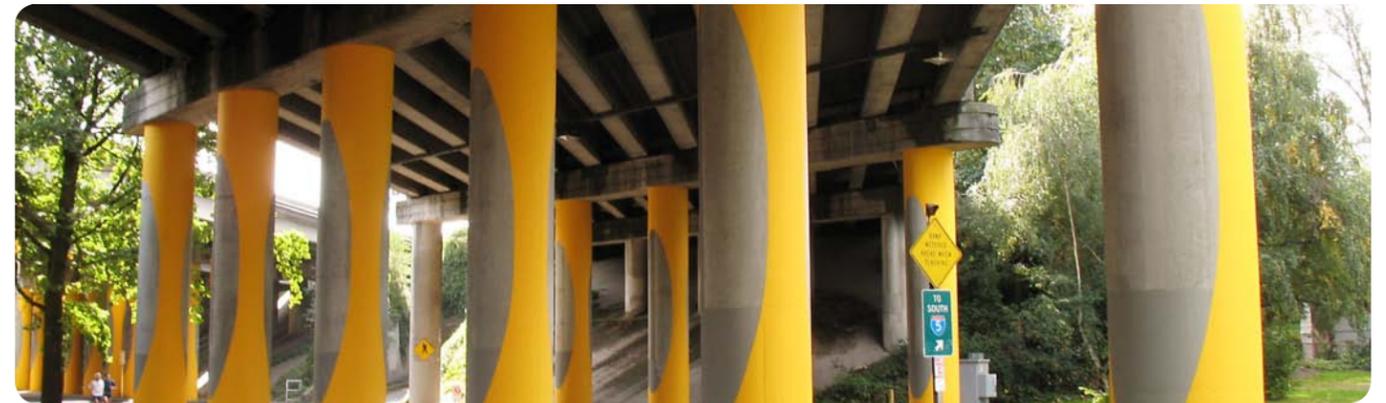
Colorful columns with graphic designs create a sense of character under this freeway.

PRECEDENTS

The columns under the bridges offer ways to add artistic elements that relate to the local identity of their neighborhoods tying them back to their communities, putting them into a specific context.



Colorful murals in analogous colors liven up the heavy columns.



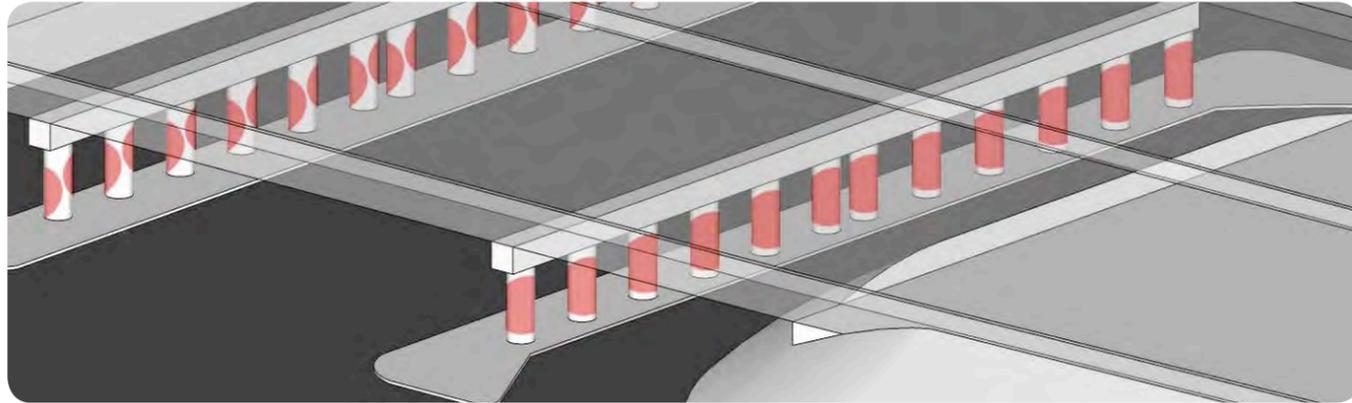
Simple shapes painted on the columns present an effective way to define space.



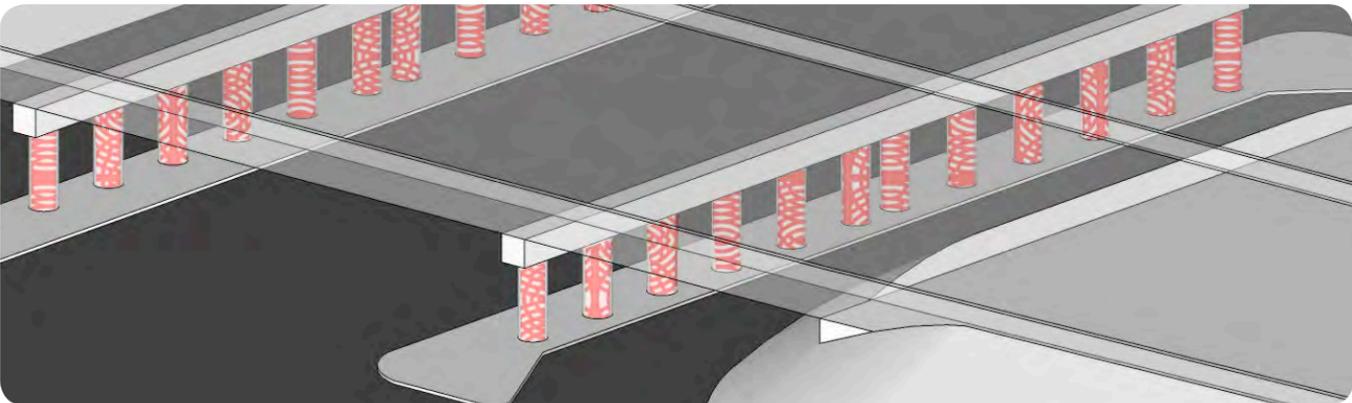
El Paso's own Chicano Park demonstrates an example of how adding artistic elements to columns can transform a space.

COLUMNS

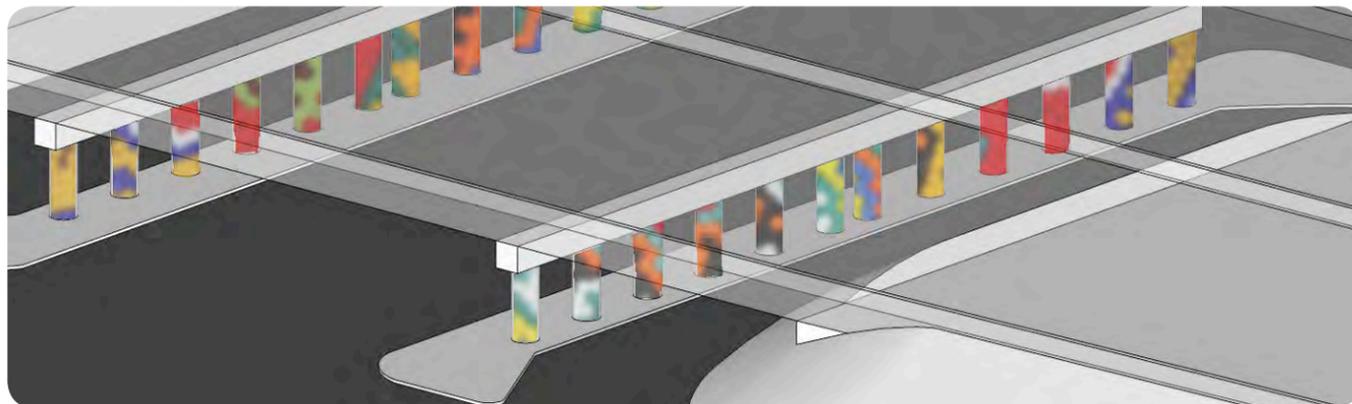
VARIATIONS



Simple shapes on each column create a graphic pattern and create visual hierarchy.



Patterns wrapped around each column in cut metal or paint relate to local identity.



Murals on all or some of the columns enliven the space and reflect local motifs.

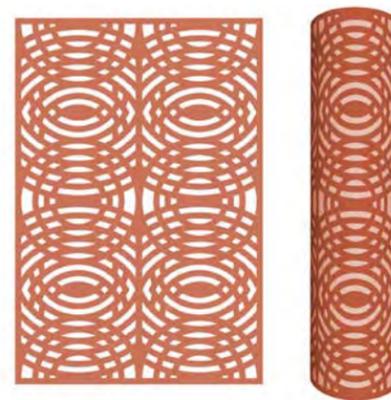
COMPOSITE



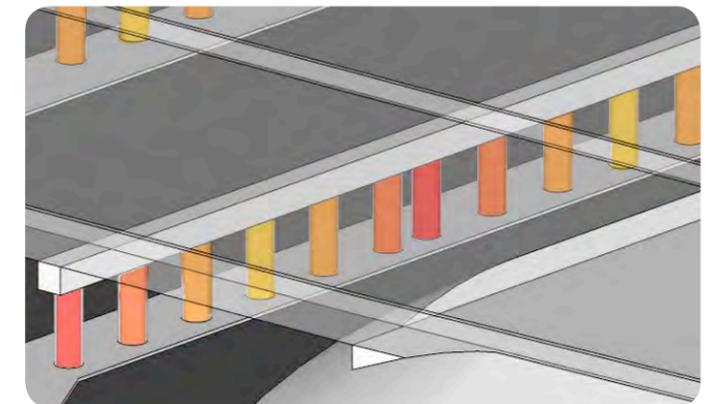
The dingy grey environment of the underside of the bridge feels stark and uninviting. Adding color and patterns to the columns brightens this mood.



Patterns inspired by local influences wrapped around the columns add visual interest and help to enliven the under bridge experience.



Wrapping patterns around the columns creates a three dimensional effect that changes as one moves around it.



The columns, while being a part of a larger family, could vary from each other, alternating in a pattern.

FACADES



Introducing artistic elements makes a simple bridge facade more interesting.



Intricate pattern work achieved in cut metal strongly reflects the local identity.



The airy feeling of this metal work reads as both subtle and powerful.

PRECEDENTS

The bridge facades offer opportunities to add artistic elements that relate to the local identity of the neighborhood tying the bridge back to the community and putting it into a specific context.



Cut metal screens feel elegant and dramatic, especially when lighted at night.



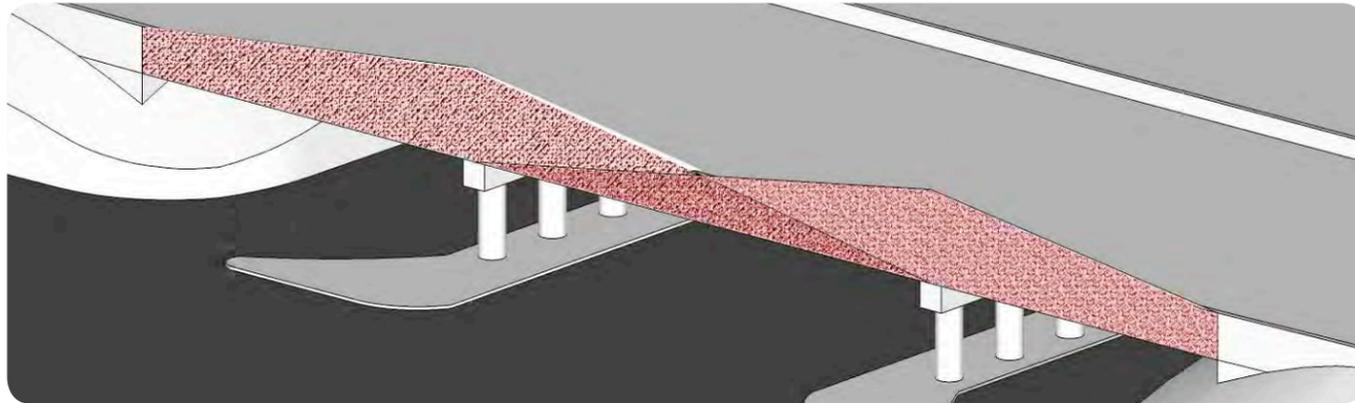
Even simple gestures redefine the expression of a bridge.



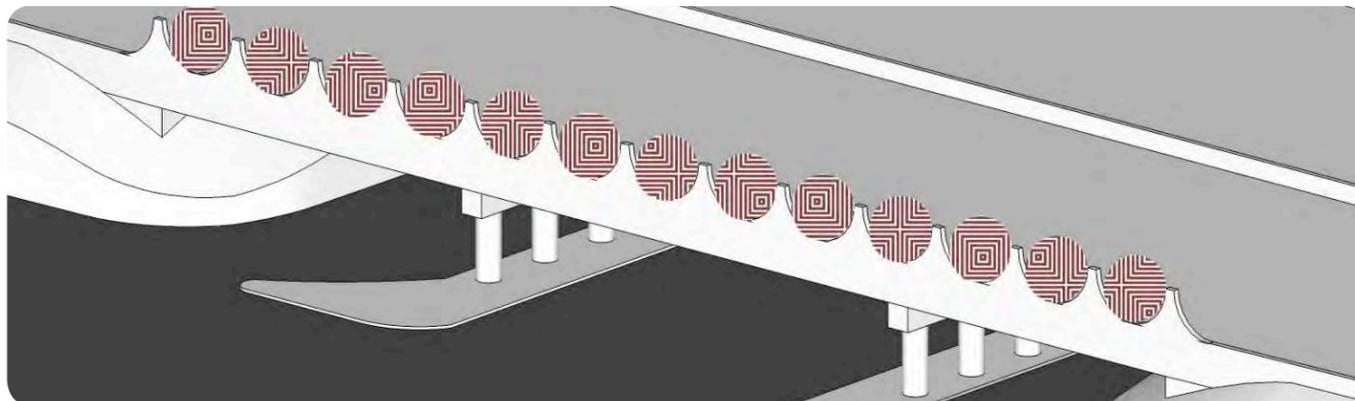
Simple painted gestures serve to obscure unsightly infrastructure and enhance the sense of place.

FACADES

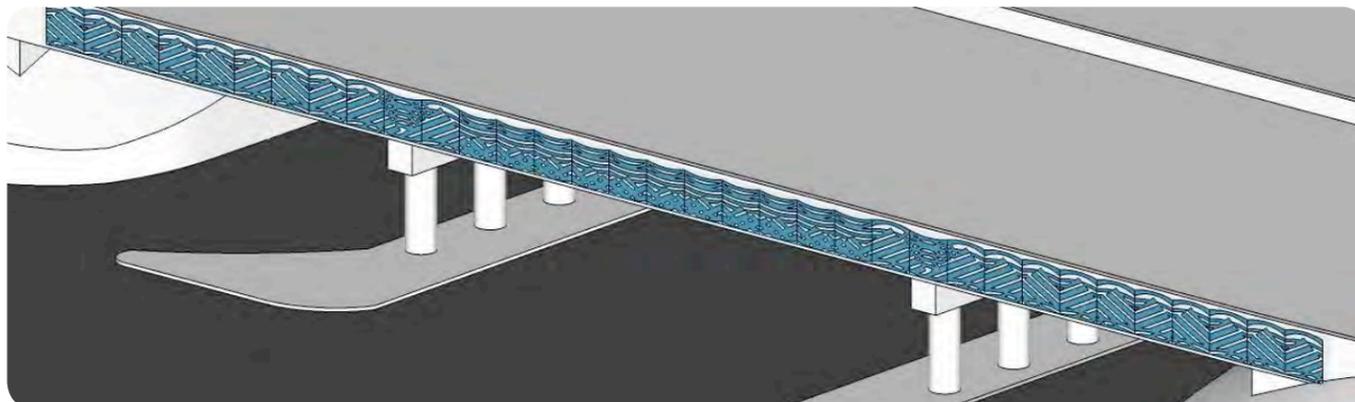
VARIATIONS



Mesh screens placed on the bridge facade change the silhouette of the span and create a sense of depth through layering of texture and shadow.



Pouring a new barrier with shapes taken from local context gives the bridge a strong graphic form and could provide locations for more art elements, such as medallions.



Keeping the existing profile but adding paint, mesh, or a cut metal pattern screen as shown represents the simplest way to add interest to the bridge facade.

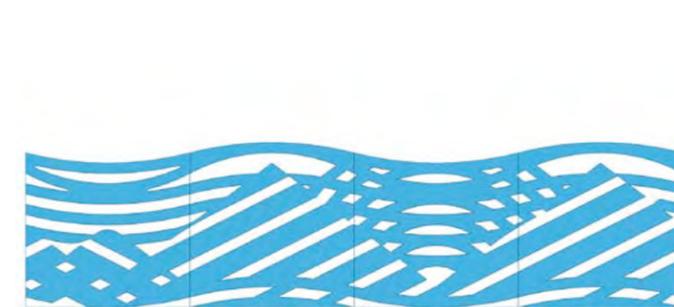
COMPOSITE



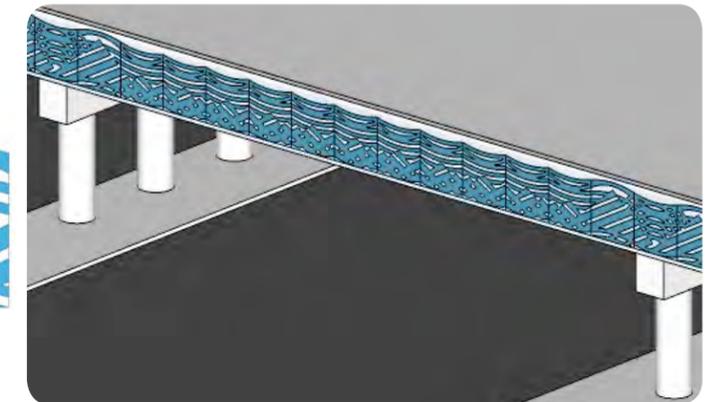
The existing bridge facade feels dull and cluttered with conduit; it has no distinguishing characteristics.



A cut metal screen in a pattern adds textural interest while unifying the bridge facade.



A pattern must relate back to the local identity of the bridge's neighborhood



The perception of the bridge span changes with the application of a simple gesture.

PAINTING

OPPORTUNITIES



The MSE walls represent a large blank canvas for a subtle painting approach in earth tones that ground the otherwise oppressive walls to the site.



Painting under the bridge lightens the mood of the space and highlights the area over the pedestrian path.



Painting the entire underside of the bridge and patterning the columns dramatically improves the feeling of the space.

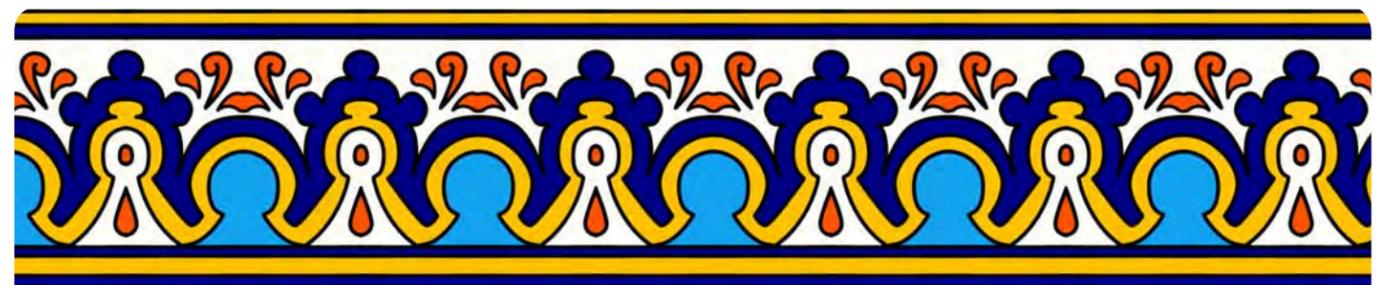
OPPORTUNITIES



Mineral Life concrete finishes come in a wide range of color choices.



Regional slope paving shows a precedent for painting highway scaled, large graphic images with a local resonance.



This pattern represents an example of possible patterning with regional themes.

LIGHTING



A lighting scheme that lights the whole space creating a general glow with specific emphasis on the walkway makes the underside a bridge feel safer.



Lighting up towards the ceiling establishes a specific mood.



Mesh elements that catch the light provide a dramatic overhead element.

PRECEDENTS

The lighting plays a large role in redefining the feel of the space under the bridge, especially at night. A thorough lighting strategy improves the experience under each bridge for drivers and pedestrians alike.



LED lighting creates a dramatic visual experience that provides interest to infrastructure elements.



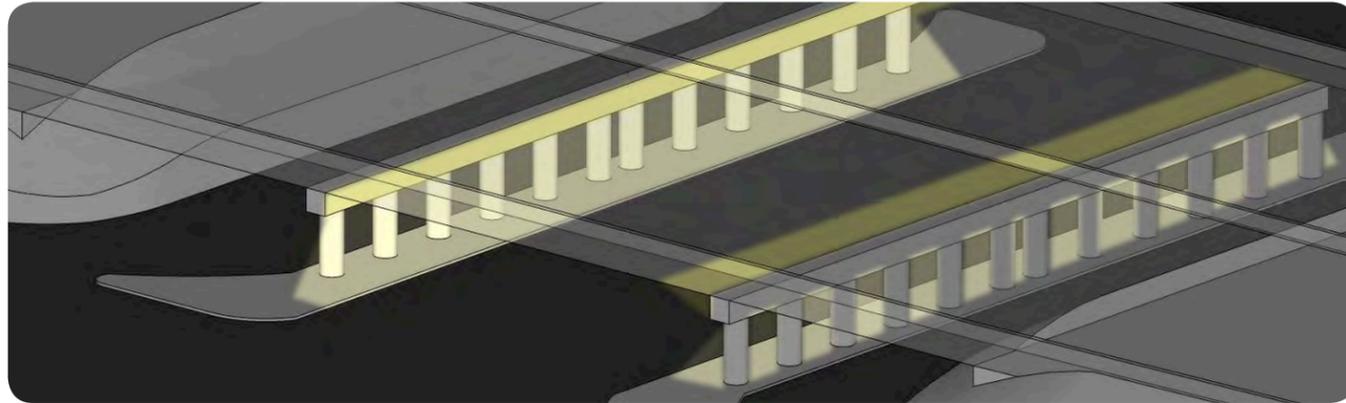
Colored lighting on columns helps to evoke a sense of identity.



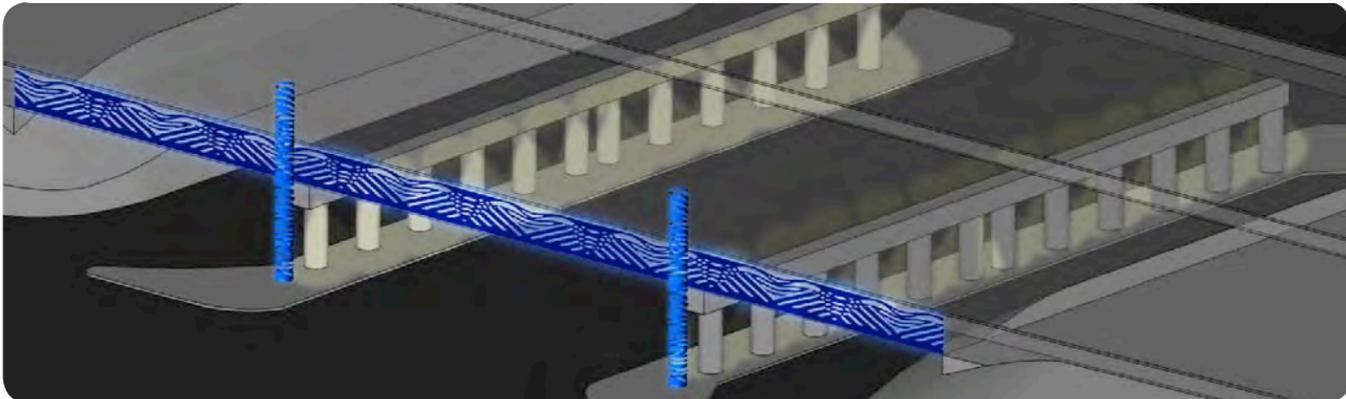
General safety lighting coupled with a combination of colored mood lighting and artistic lighting elements transforms the underside of a bridge into a welcoming place.

LIGHTING

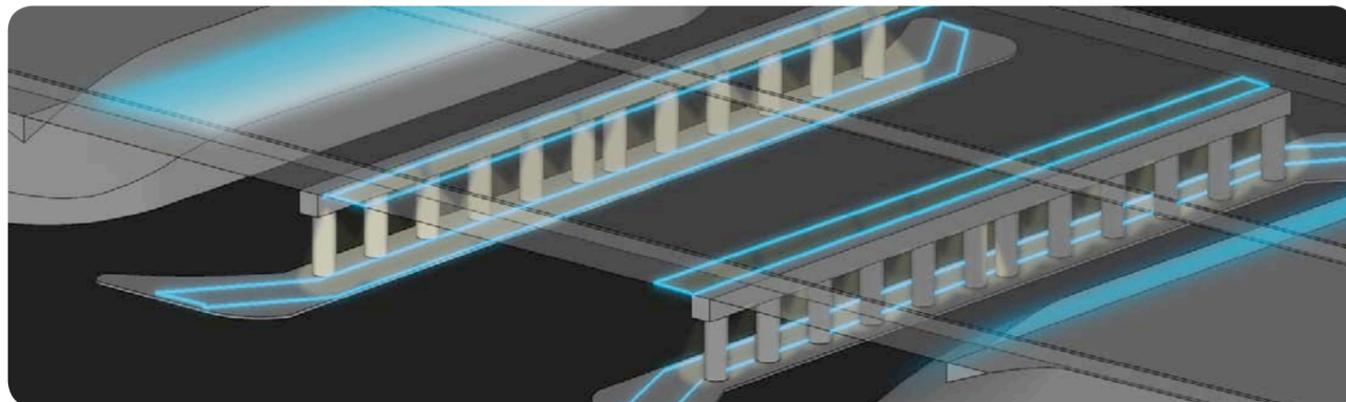
VARIATIONS



Lines of light along the infrastructure enhance and define the pedestrian area. Any lighting scheme must take into account the vertical clearance under the bridge, which restricts additional elements at many locations.



Light towers create markers at the bridges that are identity beacons, visible from a distance in the night's sky.



Lining the pedestrian area with LED stripes defines the space and creates a contemporary feeling and washing the slope paving under the bridge in colored light creates a sense of drama.

COMPOSITE



The existing lighting creates a dim poorly defined space under the bridge at night.



A lighting scheme with more pedestrian lighting and a focal element improves the safety and desirability of the space.



LED lighting linear fixtures allow for a range of color choices and even programmable color changing options.

WALKWAYS



Large bands of color not only create patterns on the ground plane but also direct the flow of movement across the space.



Interesting textures and patterns add a sense of history to the ground plane and serve as safety elements letting pedestrians know about edge changes.



Artistic elements embedded into the walking surface make even simple sidewalks whimsical experiences.

PRECEDENTS

The walkways surrounding the bridges and their intersections need repairing and pose an opportunity to add human scaled elements to improve the pedestrian experience for those using the sidewalks.



Stamped and colored concrete adds pattern and interest to the ground plane without the effort of placing real brick or pavers.



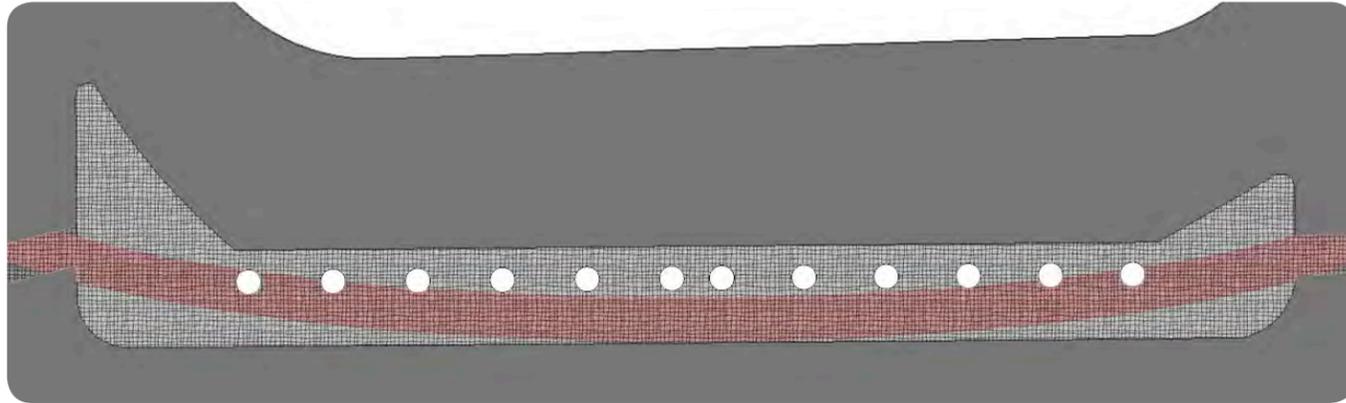
Simple forms, when carefully considered and elegantly arranged, create powerful spaces.



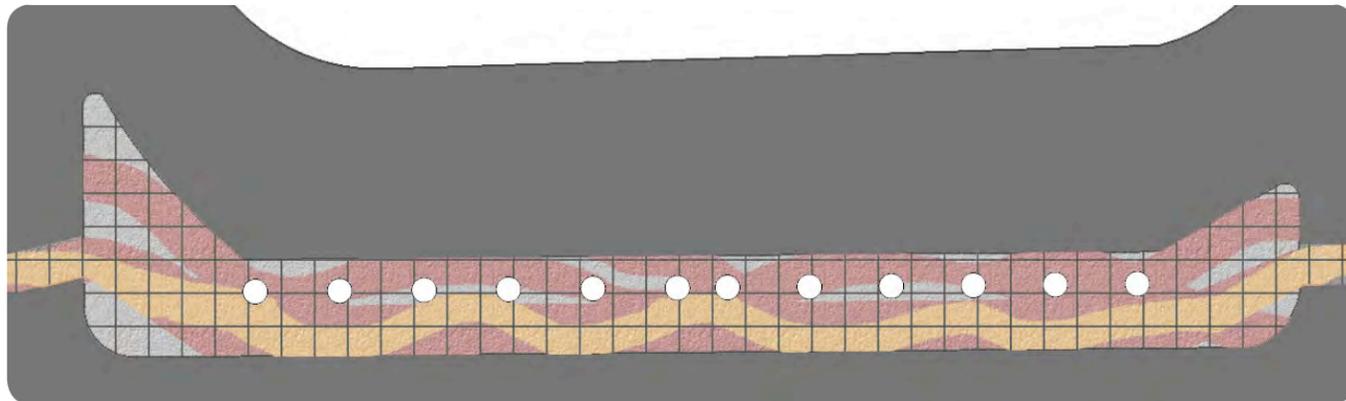
Larger pattern elements formed into the walking surface help to establish local identity on the ground plane.

WALKWAYS

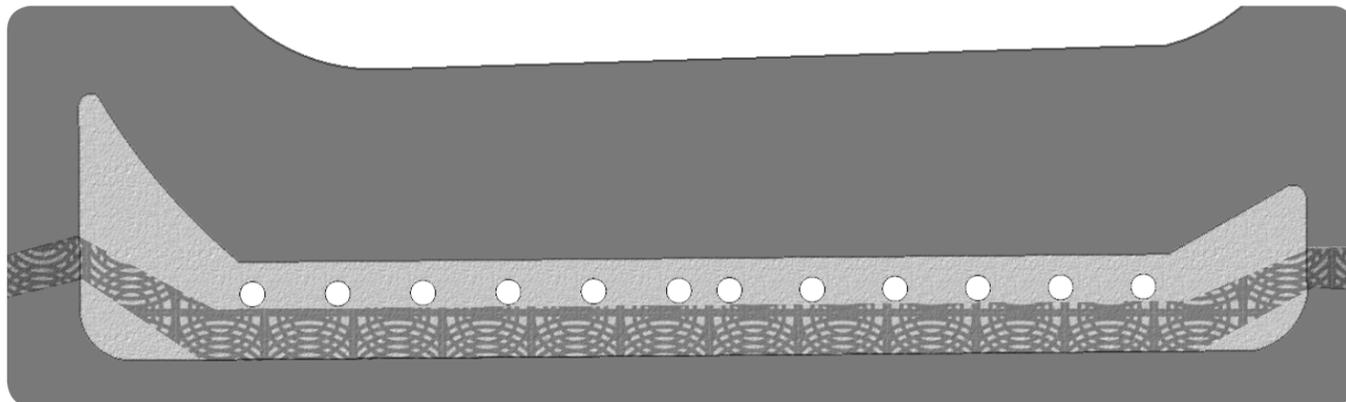
VARIATIONS



Patterns in the ground plane made from brick remain durable and easy to maintain.



Patterns made from poured concrete make larger gestures and reform the cracked walkways.



Design patterns add a more graphic punch to the ground plane under the bridge.

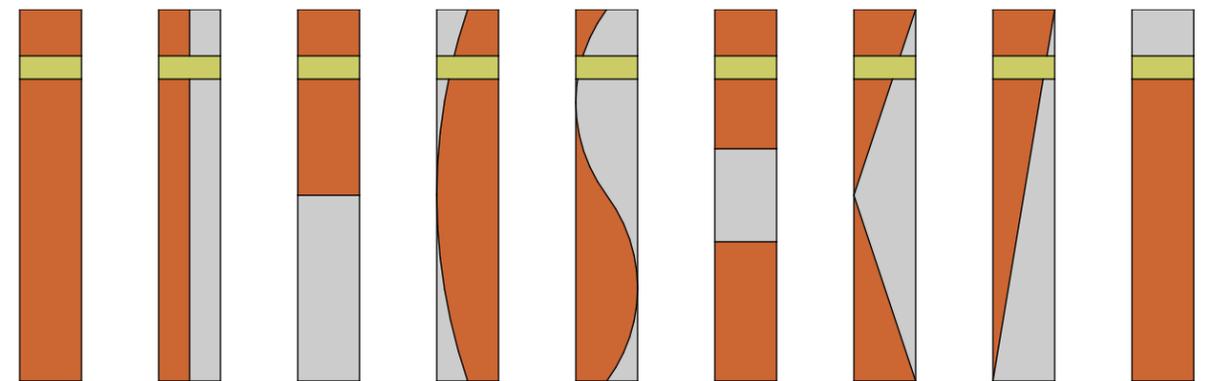
COMPOSITE



The existing pedestrian experience feels unsafe and uninviting with an ill-defined walkway and the cars seeming too close to pedestrians.



Incorporating patterning into the ground plane, clearly defining the pedestrian area and adding lighted bollards increases the sense of safety.



The lighted bollards create a sense of safety while defining the pedestrian area and they can also be painted many colors or painted in different patterns.

ABUTMENTS



Simply painting high contrast patterns on the abutments walls represents a good way to add interest.



A strong composition of stones creates a smooth refined effect that pulls the abutment together and adds a sense of scale.



Large graphic identity elements take the sun and add a strong textural interest.

PRECEDENTS

The abutments, whether left intact or replaced with cast in place in new construction, offer locations to place aesthetic elements adding interest to the underside of the bridge.



The painted abutment reads as just a flat surface that does not play with light, shadow or texture.



Patterning abutment walls adds visual interest under the bridge, opening it up.



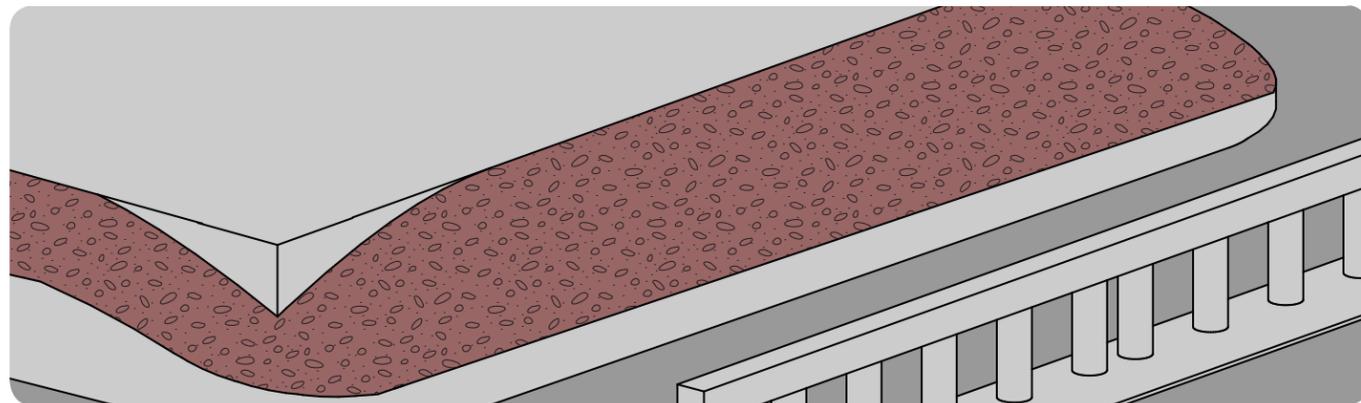
Concrete patterning on the wall surface unites the length of an abutment wall.

ABUTMENTS

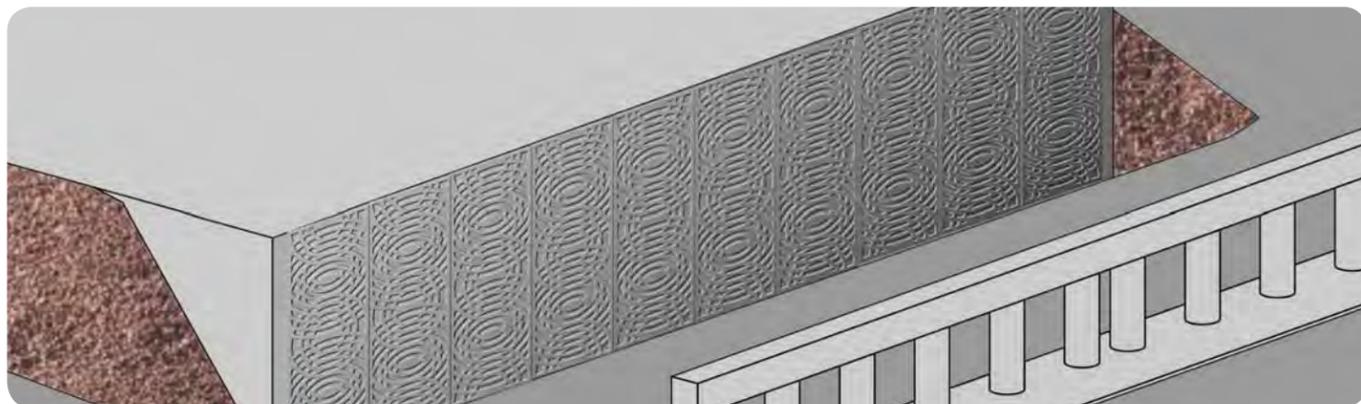
VARIATIONS



Painting the existing slope paving with a graphic pattern inspired by local geometries helps to liven the underside of the bridge while relating it back to a sense of place.



Covering the slope paving with colored rock aggregate ties it into the larger landscaping scheme and add textural interest to the underside of the bridge.



At locations where new bridges are necessary, building a cast in place wall adds more space and, with patterned walls, adds artistic interest to the underside of the bridge.

COMPOSITE



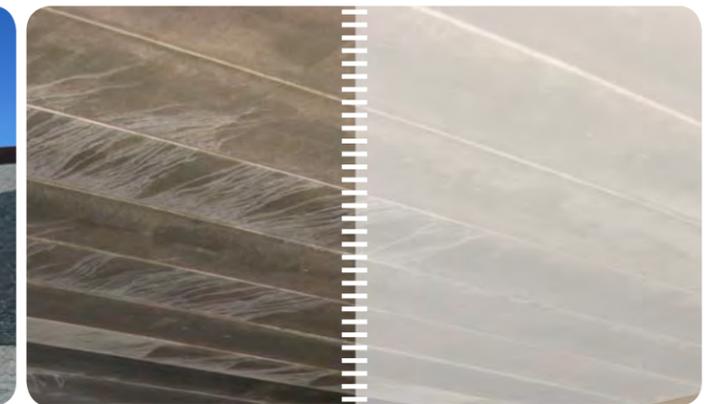
The underside of the bridge with wide barren abutments feels dark and gloomy.



Livening up the abutments with patterns in paint or rock aggregate improves the experience under the bridge for both drivers and pedestrians.



Colored rock aggregate creates clean controlled high contrast patterns.



Regardless if the abutments get painted, covered in rock or replaced; the underside of the bridge receives a lighter paint color.

WALLS



Incorporating local textures, materials and pattern elements into walls can help establish an unmistakable sense of place.



More abstract pattern work can also create a specific aesthetic feel for an otherwise plain stretch of wall.



Environmentally scaled artwork provides a sense of scale and drama for viewing at a variety of speeds and distances.

PRECEDENTS

Walls are highly visible surfaces for aesthetic improvements. However, whatever is done to them must not be overwhelming or tiring over long expanses.



Concrete pattern relief is enhanced by landscape with the play of light and shadow.



VS SiteWorks has a long and successful history of using form liners to create patterns in concrete walls, demonstrated by this MSE wall in Shoreline, WA.



Walls are a perfect location to present the work of local young artists who may draw influence both from the fine arts and a street art aesthetic.

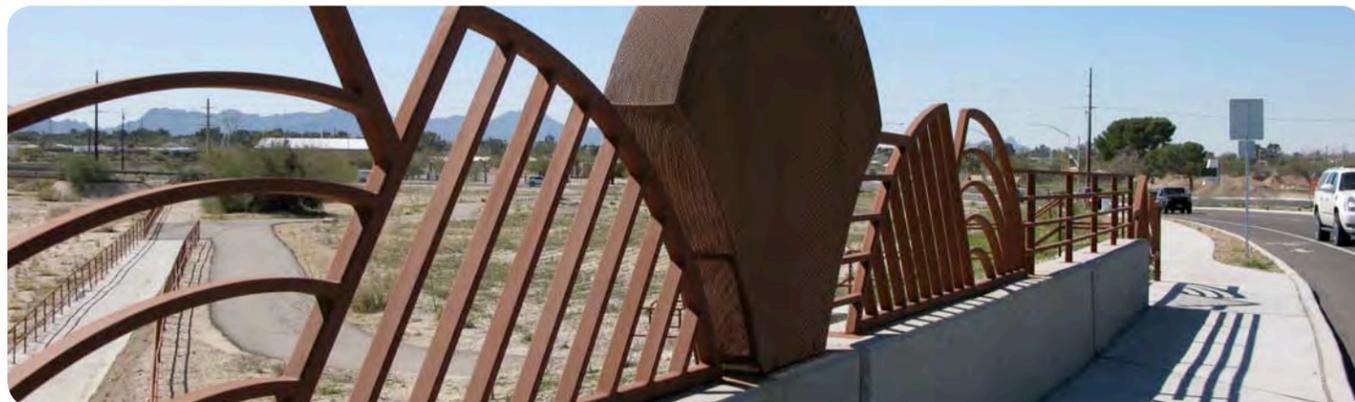
RAILINGS



Simply cladding the existing infrastructure with curving gestures in metal transforms a bridge's feel.



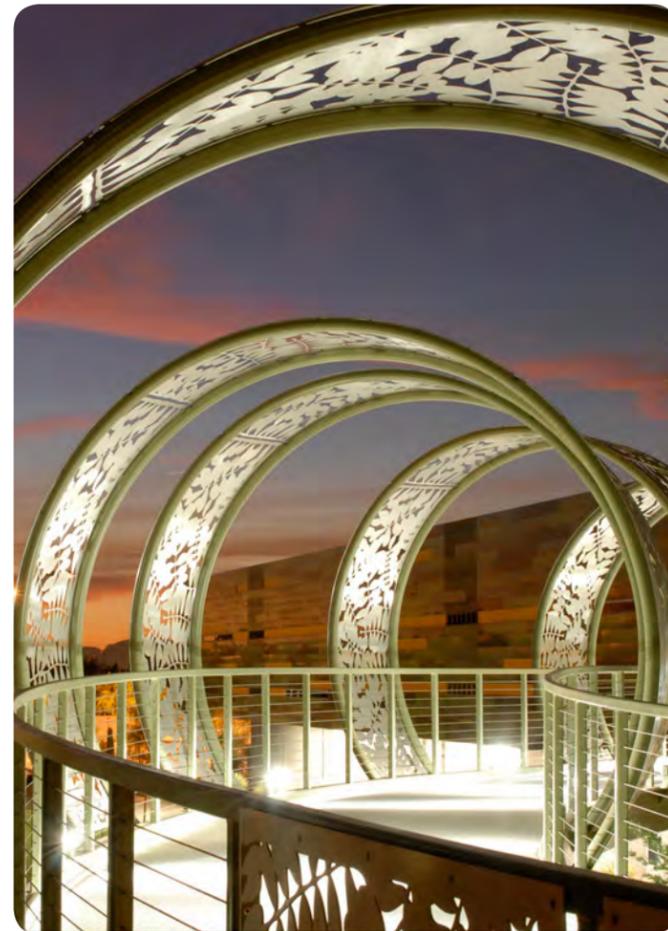
Dramatic gestures in the railing create beautiful patterns on the bridge and in the shadows.



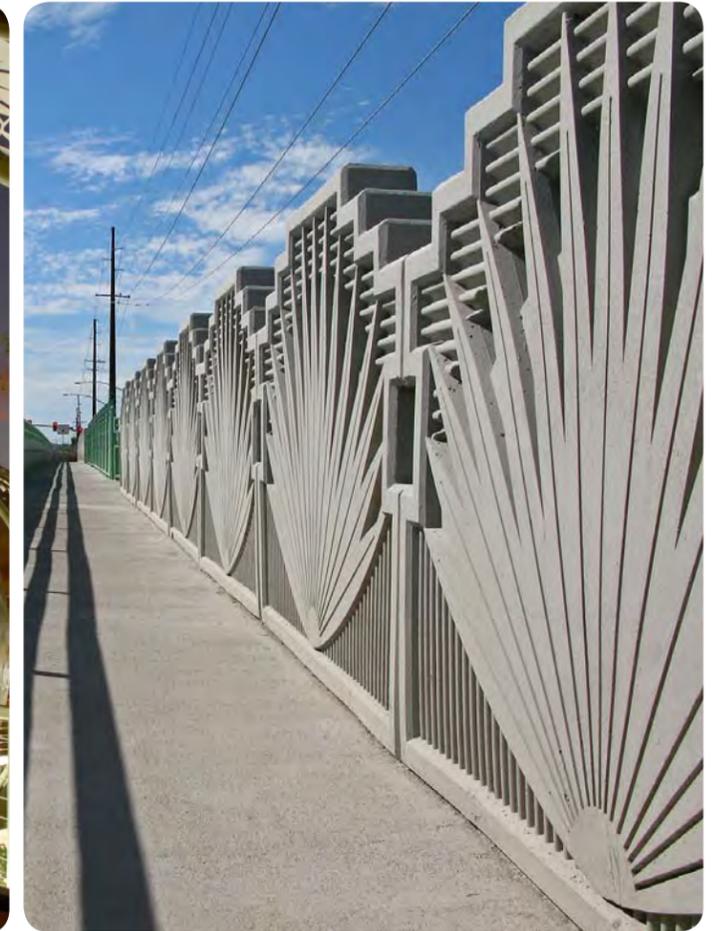
Strong geometric shapes and forms in materials that relate to the local identity relate a bridge back to a sense of place.

PRECEDENTS

The bridge railings downtown demand replacement so as to speak to the character of the place and feel appropriate in establishing the importance of the downtown through pattern, color and form.



Simple railings coupled with dramatic cut metal elements and lighting create a visually compelling space on a bridge.



Detailed concrete form work helps pull each element of a bridge structure together to create a designed whole.



Brightly colored metal patterns with strong local themes represent a community on its own bridges.

RAILINGS

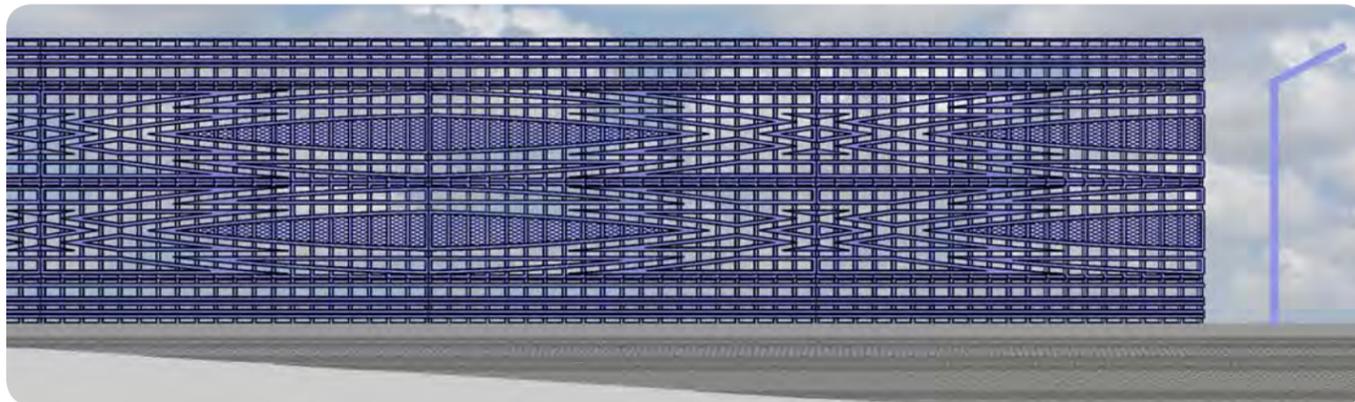
VARIATIONS



Cut metal pattern panels in traditional El Paso colors work as railing screens that tie the bridge back to local heritage.



A railing of shaped concrete with pattern motif medallions holding local design elements creates a colorful border that frames views of the downtown.



A delicate screen of metal elements references El Paso's railroad history.

COMPOSITE



The existing chain link railings do nothing to establish a sense of local identity and actually diminish the aesthetic effect of the downtown corridor.



Introducing a polychrome system of patterns and symbols relates the bridges back to the neighborhoods that they connect and ties them into the larger downtown scheme.

HODGE-PODGE

Too much, too little ... this is a good example of too many expressions pieced together, creating a cluttered appearance that does not transform the infrastructure.



This project is an excellent example of hodge-podge. It shows too many statements without an overall idea. The result is a cluttered expression that does not transform the site.



This stretch of wall flanks this interchange. While the parts are attractive the whole does not add up.



Images of local culture are included on the walls. While the tile mural images are interesting, they do not work in the site.



Along with the murals, these wave forms mark the abutment walls. They seem out of context with the artwork.



Painted graphics reinforce shallow relief. This creates a stuck-on look.



Cultural influences may provide interesting patterns and designs, but here they collide.



Tile murals feature digital imagery. The murals are applied, not integrated with site.



Tile murals can provide visual interest and low maintenance. It is key to work with the scale of the space and the site.



This artwork is located across from the interchange. It introduces yet another theme that is not supported.

PART 3: LOCAL PATTERNS

Local patterns provide the inspiration for the pattern work in the design plan. Landscape, facades, columns, walkways, abutments, and railing patterns derive from local influences.



The indigenous patterns found at the Hueco tanks offer ancient precedents.



Aztec patterning has a strong connection to Latino heritage.



El Paso's Art Deco heritage provides many great patterns.



Architectural details often inform other pattern opportunities.



Local plant motifs add representational ornamentation.



Plant motifs can also feel more abstract.



Color and pattern often work together to create visual interest.



Contemporary shapes in industrial materials give a very elegant expression



Pattern inspirations can derive from local neighborhoods.



Even simple brickwork offers a dynamic pattern.



Local influences include simplified pattern motifs of detailed imagery.



Simple forms can create more complex and detailed patterns.

LOCAL IDENTITY: EL PASO

Colors, textures, patterns, and multicultural influences are identified with El Paso and its neighboring regions.



The Franklin Mountains are full of native flora and fauna despite the hot and arid conditions.



The Annual Franklin Mountains Poppies Preservation Celebration brings a burst of color and beauty every spring.



Colorful prehistoric patterns decorate Hueco Tanks rock walls.



Rock formations provide a palette of textures for inspiration.



Regional architecture gives us a glimpse of the city's history.



Architecture can also be an eclectic mixture of two cultures such as can be found at the University of Texas at El Paso.



The mountain star and sunset are perhaps the most prominent features after the Franklin Mountains.



Historical venues not only provide a location to perform, they provide beauty and history.



Cultural influences provide interesting patterns and designs.



Tigua Indians also influence this region's history.



Regional crops, such as dry chiles, are commonly seen in many homes.



Colors and patterns can be found enlivening neighborhoods across the city.

LOCAL IDENTITY: WROUGHT IRON

Wrought iron fences are commonly found in the Geronimo and Trowbridge neighborhoods. These fences display many patterns providing inspiration for geometric motifs.



Fences with circular borders protect this yard and home.



Floral burst fencing in bright pink is festive and eye catching.



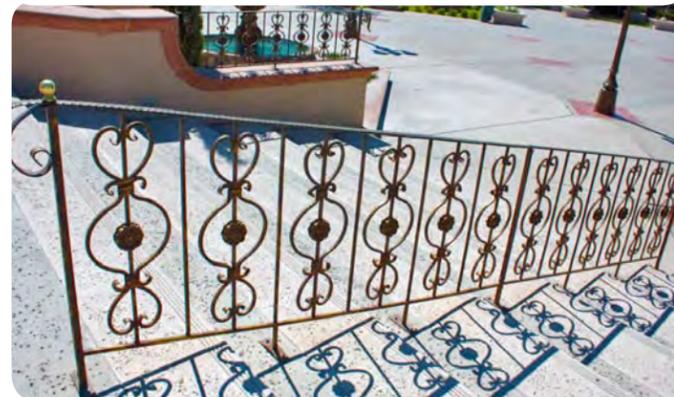
Floral patterns add character to this fence.



Long sweeping curves are organic despite the industrial material.



A simple and bold design pops in contrasts to its surroundings.



Simple and elegant banisters are used at the nearby church courtyards.



A more ornate combination of fencing and gates adds elegance to this brick wall.



A simple design in a bright color is more than enough for this gated fence.



A very decorative iron element proudly crowns this rock wall.



A complicated border adds so much to straight iron rods.



A combination of a floral burst with a curled top border is both whimsical and geometric.



Curves soften repetitive vertical lines in this tall fence.

LOCAL IDENTITY: HAWKINS

Hawkins, most identified with Cielo Vista Mall, combines a predominantly business and retail zone with single family homes and nearby apartment buildings.



Public art, in regional motifs and colors, decorates the entrance of the José Cisneros Cielo Vista Public Library.



The architecture of Cielo Vista Mall, the biggest and busiest mall in the city, reflects that of old haciendas with ceramic shingles.



Many trucking and other industrial warehouses sprawl south of the highway.



Street art adds color and life to this stark warehouse area.



Other businesses cluster around the mall; including a movie theater, restaurants and supermarkets.



Public transport placed one of its main terminals near the mall making this area traffic heavy and very busy.



This neighborhood houses El Paso Community College's main campus.



A mural of a mission slowly weathering away on EPCC campus grounds displays the area's predominant faith.



Gracious homes populate this location.



Well designed and cared for medians sit on the north side of the interstate.



Small parks, located here and in other surrounding neighborhoods, offer people space to gather and exercise.



Cielo Vista Mall's mandala-like emblem viewed from the highway displays a colorful regional theme.

LOCAL IDENTITY: AIRWAY

Airway, the main road to the El Paso International Airport, welcomes visitors with many hotels, restaurants, car dealerships, and a large equestrian statue.



Airway welcomes many visitors flying to El Paso in its role as an entrance zone.



The larger than life equestrian statue, a local landmark, greets visitors.



A locally owned restaurant artistically showcases humorous characters on their front facade.



Despite its abandoned look, many people enjoy the nighttime entertainment here.



The airport draws attention as the most prominent feature on Airway.



Hotels wait nearby the airport for visitors' convenience, rest and relaxation.



The El Paso Comic Strip Comedy Club brings many entertainers to this region.



Most of Airway consists of numerous restaurants and small shops.



Edgemere Park, a median located park, provides a place to get away for those living nearby.



Modest apartments are tucked into the neighborhood.



Car dealerships abound in this area.



GECU bank headquarters displays a unique angular structure.

LOCAL IDENTITY: GERONIMO

Geronimo, home to Basset Place, surrounded by commerce, car dealerships and modest residential neighborhoods attracts many locals and visitors for shopping.



Basset Place is the biggest shopping location in this area after Cielo Vista Mall.



A wholesale market and big box retailers attract many people to the area.



Majority of car dealerships locate themselves around this area.



Trade and vocational schools are located near Geronimo.



Outside the mall cluster many small retailers for consumers' convenience.



Coffee shops offer a place to unwind for vocational students and employees of surrounding businesses.



Iron works are a dominant local art form.



Homes are small and modest.



Some stores and restaurants have a retro feel to them.



A colorful mural depicting El Paso's history graces the street passing by the mall.



St. Pius X Catholic Church provides schooling and a religious gathering place for this neighborhood.



Regional patterns, colors and culture enliven this area.

LOCAL IDENTITY: TROWBRIDGE

Trowbridge occupies both a residential and industrial zone. It is one of the largest interchanges on the I-10 corridor challenging its neighborhood with its size.



The Western Refining plant refines crude oil into gasoline occupying a large tract of land.



Tall metallic towers and smoke stacks line the street reinforcing the industrial character.



Ascarate Park was once home of El Paso's only amusement Park, Western Playland.



Ascarate Park is open to the public for picnics near the lake. Fishing and swimming are not advised.



Freeport-McMoRan, once a Phelps Dodge smelting plant, makes its home in the industrial zone.



Despite the heavy industry, ample trees and shrubs soften the impact of these factories.



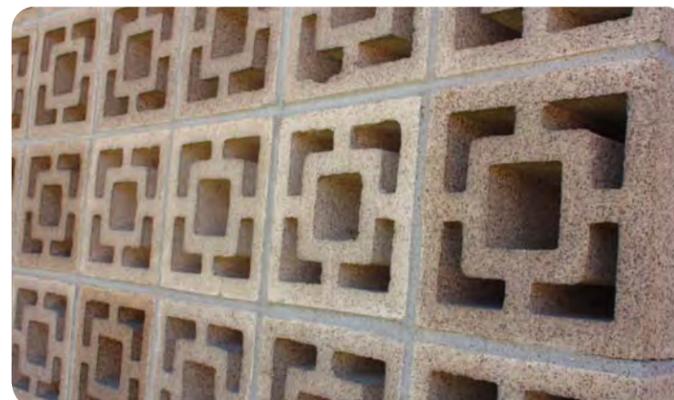
The remains of the Ascarate Drive-In Theater house the Ascarate Flea Market.



Ascarate Drive-In opened in 1958 and closed in 1988. It stands as a skeletal ruin.



Small, modest homes line the neighborhood.



Cinder block patterns define some walls in this neighborhood.



Loretto Academy, a private Catholic school, makes a strong visual impact in the area.



Nazareth Hall Nursing Center utilizes traditional designs in a local palette.

LOCAL IDENTITY: PAISANO

Paisano, a commercial/industrial area, acts as an entryway to city attractions and entertainment venues offering diverse cultural activities.



The recreation center displays vibrant hues and a colorful mural.



Nearby neighborhoods feature lively street art creating local character.



The Chamizal National Memorial Park, great for family events, hosts picnics and concerts.



People shop over at the nearby mini-mall for a variety of products.



The community offers a variety of programs for the local youth.



The Salvation Army aids those in need.



Many locally owned auto repair and body shops boast vivid signage.



An abundance of Local Mexican restaurants feed this neighborhood.



The El Paso Zoo is a point of interest for city residents and visitors alike.



The El Paso County Coliseum holds many entertaining events.



El Paso's first medical school stands here.



Bright and vividly painted buildings animate the neighborhood.

LOCAL IDENTITY: CHELSEA

Chelsea, a mostly residential zone, includes auto repair shops, the Fox Plaza Flea Market and many local businesses. The University Medical Center of El Paso and Texas Tech University Health Sciences Center anchor this neighborhood.



Most homes are small and modest but each is unique in its appearance.



Some homes are painted in bright colors. This one features a Texas star and a fountain, giving it local flare.



Smaller businesses lay near the tracks crossing Chelsea.



The University Medical Center of El Paso and Texas Tech University Health Sciences Center at El Paso are nearby.



Cultural icons are apparent throughout the neighborhood, such as alters.



The nearby fire station has regional design.



Businesses are painted in vivid colors to attract attention.



On Sunday mornings, many El Pasoans gather to shop at the Fox Plaza Flea Market.



Chelsea meets an abrupt end as Fort Bliss begins, marked by a tall fence.



Single family homes and a multi-story retirement home are close to the interstate.



A hot pink bus stop is located close to the retirement home, promoting transit use.



There are many auto repair and auto body shops in Chelsea.

LOCAL IDENTITY: COPIA

Copia, a diverse neighborhood of commerce, residences and industry, sits next to the Spaghetti Bowl, one of the largest freeway interchanges along I-10. Copia is well known for its beloved Lincoln Park sitting beneath the infrastructure.



At Copia, the area close to the railroad tracks is desolate and dusty with unpaved roads.



Much of Copia is an industrial warehouse district with a variety of small businesses.



Lincoln Park is a Mecca for Chicano counter-culture gatherings displaying community pride and heritage.



The local community has cleaned up Lincoln Park and decorated its columns with images celebrating their cultural heritage.



Copia neighborhood businesses are predominantly Hispanic owned.



Copia houses many small, family-owned businesses.



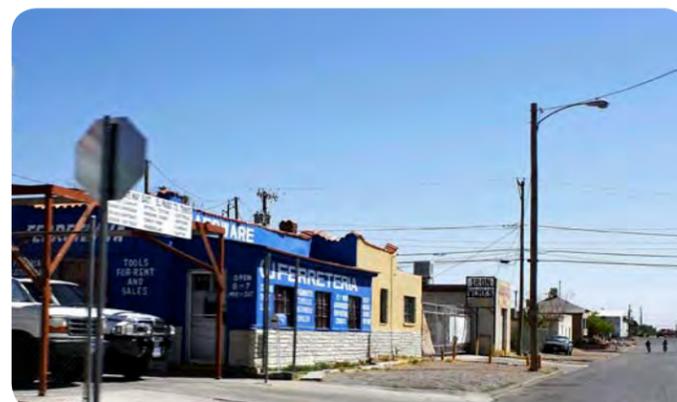
Lowrider bicycle competitions are often held at Lincoln Park.



Lowrider car competitions are often held at Lincoln Park.



This neighborhood has several small shops and bars frequented by locals.



This district is known for the many local automotive and mechanic shops.



The columns under the freeway overpass are painted with beautiful murals that represent Chicano culture.



The people who gather at Lincoln Park create a sense of community with their clubs and associations.

LOCAL IDENTITY: PIEDRAS

Piedras is a very colorful neighborhood. Enlivened with pedestrians, automobiles and local businesses, Piedras is an up-and-coming neighborhood with a promising future.



Prices Dairy plant, located in Piedras, is a big local business.



There are many retail and commercial shops in the Piedras neighborhood.



South on Piedras leads to Alameda. On Alameda landscaped medians enhance the streetscape and neighborhood.



Along Alameda, stamped concrete and benches line the sidewalk edges, adding pedestrian scale and amenity.



Manhattan Heights is located further north on Piedras. It began as a copper smelting company; development followed.



Most of the large homes in Manhattan Heights were built at the turn of the 20th Century for prominent residents.



Alameda is bustling with pedestrians, automobiles and brightly colored businesses.



Piedras and Alameda are an up-and-coming areas due to their close proximity to Texas Tech and a new hospital.



Memorial Park has acres of rolling hills, lush lawns and many trees. It is famous for picnics and barbecue gatherings.



Weddings and special events take place at the Rose Garden next to Memorial Park.



Many small, colorful, local businesses add character and charm, creating sense of place.



The demographic of the area is lower middle class families. Yet the homes are extremely well kept and maintained.

LOCAL IDENTITY: COTTON

Cotton Street sits between contrasting neighborhoods of residences, commerce and industry. The area is primarily known for the large railroad yard running along the I-10.



The railroad yard is immediately east of I-10 on Cotton.



Bridge fencing displays railroad imagery visible through the play of light and shadow.



Printing presses and newspaper offices, such as El Diario Newspaper, are located in the Cotton neighborhood.



Cotton is home to many small businesses occupying low-rise commercial buildings near the railroad.



The overpass bridge at Cotton is supported by a forest of columns, creating interest.



The railroad area near Cotton is industrial and desolate, awaiting redevelopment.



Magoffin Historic District preserves this neighborhood's authenticity.



Low-rise commercial buildings add human scale to the streetscape, softening the impact of the railroad infrastructure.



Factories & wholesalers occupy warehouses, such as the Sunbeam Bread Factory.



Glass Box Contemporary Gallery and artists' studios are housed in a former foam factory.



The Magoffin District is a mostly residential neighborhood.



The Magoffin District values its historic buildings and churches.

LOCAL IDENTITY: DOWNTOWN

The Downtown is considered the heart of El Paso. Its rich history is seen in its fine historic buildings, recalling its role as a center of trade and commerce in the early 20th century. Now, El Paso is entering a new period of redevelopment.



The plaza is a popular gathering place. A Luis Jimenez alligator sculpture recalls the live alligators which once lived at the park.



Downtown developed in the early 20th Century. Many downtown buildings are considered historic landmarks.



Most government buildings are housed downtown. Municipal, state and federal courthouses sit in this district.



Large corporations and small businesses reign downtown.



The renovation of older buildings is turning Union Plaza into a thriving neighborhood.



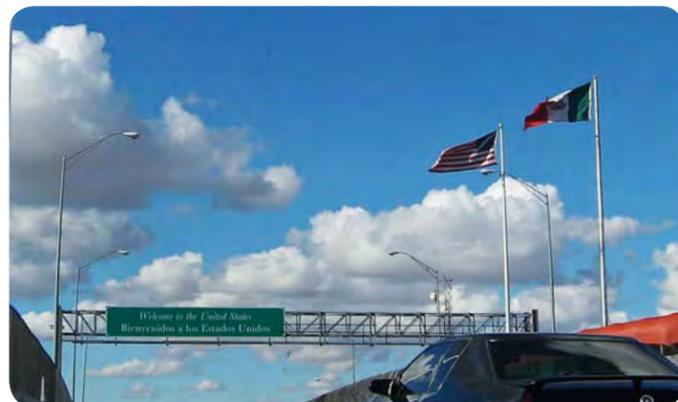
Union Plaza is alive with night life, social scene, art, and sense of community.



Festivals, cultural events and conferences take place at the Abraham Chavez Theatre.



Most museums and performance art venues are housed downtown.



El Paso is a major U.S. port of entry. There are two entry bridges located downtown.



Trade of goods, business and labor passes through the border everyday, promoting influences from Mexico.



Segundo Barrio is the low income residential neighborhood within downtown.



Segundo Barrio is a strong source of Chicano identity, pride and culture.

LOCAL IDENTITY: DOWNTOWN

Past and present cultural influences, architectural details and styles add character to the downtown district. El Paso has a great stock of historic buildings awaiting redevelopment.



The classic architecture at the Cortez building is from the turn of the 20th century.



The historic clock at the downtown plaza reveals some of the city's history.



The idiosyncratic design of the Abraham Chavez Theatre adds to downtown's unique skyline.



Holes and domes at the Insights Science Museum draws attention to the sky.



Renovated historic accents keep El Paso's history alive.



Architectural terra-cotta details recall Beaux-Arts influences at the turn of the century.



Colors and arches announce the entrance to the Abraham Chavez Theatre.



Arches emphasize the El Paso Union Plaza that houses the Train Museum.



Local retailers display their wares along the streetscape blending a mix of cultures.



Buildings display many cultural influences with shapes, styles and colors in El Paso's downtown core.



Art Deco architecture from the 1930s and 1940s adds character and history to the core.



Spanish colonial architecture is found in the Sunset Heights and San Francisco Heights District near downtown.

LOCAL IDENTITY: EXECUTIVE

The Executive neighborhood stands between the Border Highway and Mesa street. Its identity is derived from its office parks and commercial buildings set against the mountains.



Commercial parks along Mesa and Executive are well maintained.



Office spaces all along Mesa and Executive are landscaped with native plants and trees.



Slope paved walls dominate the I-10 corridor near the Executive exit.



Utilitarian walls mark the railroad bridge over the I-10.



Restaurants and bars line Mesa Street near Executive. This neighborhood is very lively and predominantly commercial.



Mesa Street near Executive houses many medical practice offices.



The abandoned Asarco smelting plant towers can be seen from the Executive and I-10 intersection.



Minor landscape accents at Executive and I-10 exits dot the area.



Upscale homes reside on the mountains north of Executive.



There are several apartment buildings.



The area along the Rio Grande river's edge is very desolate yet interesting.



Mt. Cristo Rey in Sunland, NM can be seen from I-10 near Executive.

PART 4: SITE STUDIES

Along with the general design opportunities mentioned earlier, specifically commissioned and positioned art pieces will help to bring local identity into the I-10 corridor. They could be stand alone sculptures, light elements, banners, or more integrated elements like benches or planters.

The chief manner in which local identity will manifest itself in this scheme is through the use of local pattern inspirations. El Paso has a wide range of patterns to draw from including early indigenous artifacts, natural forms, Art Deco motifs, and Hispanic traditional art.

The following pages show before and after views of the proposed aesthetic improvements along the I-10 corridor. The collage composites represent schematic design ideas and are meant as inspiration for future design work and not to be interpreted as final design solutions.

Not every solution is demonstrated but the illustrated models serve to inform further design work. By using the aesthetic opportunities in concert with local influences, specific identities can be created at each bridge that also function as a part of a greater plan.



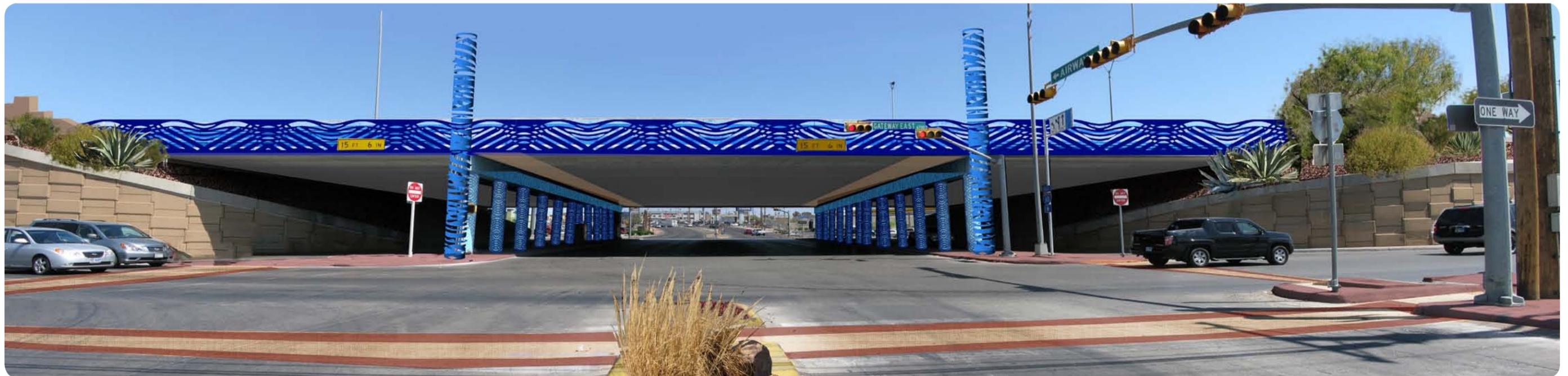
Designation: Retail District
Proposed Color: Blue
Priority for Redevelopment: Current
Estimated Cost: \$ 10 M

AIRWAY

SITE COLLAGE



Airway is an important destination, marking the route to the El Paso International Airport. Currently, it sits as a large mass of chipped gray concrete without a specific neighborhood or regional character. Conduit clutter the span. The slope paving lies barren. The underside feels undesirable. The medians are in poor shape creating an uninviting and unsafe experience for both pedestrians and motorists. Airway is a diamond in the rough, deserving a full-makeover to express its prominent role in the community for visitors and locals alike.



Airway sits in a retail district, with a special distinction that it marks the route to the El Paso International Airport. Proposed enhancements include: light columns, decorative grills, column wraps, amenity lighting, decorative paving, concrete staining, landscaping, and gravel surfacing of abutment walls and slope paving. Airway, designated as blue, references the sky and flight. Light columns provide destination markers visible from near and far, promoting way-finding and identity.

Designation: Business Core
Proposed Color: Various
Priority for Redevelopment: Current
Estimated Cost: \$ 11.7 M with \$ 32.8 M in Future Funding

DOWNTOWN

SITE COLLAGE



The existing downtown corridor cuts like a canyon through the city. The tall walls and blank slope paving make it feel more barren. The bridges do not relate to the city they connect.



By redoing the paint in more vibrant colors, and possibly commissioning local artists to re-imagine the patterns, life is added to the walls. Literally adding life in plant form to the tops of the slope paving will transform the canyon into a garden space. Adding site specific railings to the bridges makes them local identity art pieces.

DOWNTOWN

SITE COLLAGE



The narrow strip of landscaping at the top of the slope paving represents a good start. However, more must be done to transform the hard surfaces into an urban green-way.



Removing all the slope paving and replacing it with rocks and landscaping greatly improves the expression of the downtown corridor. It becomes a desirable destination and a symbol of pride in the center of the city.

DOWNTOWN

LANDSCAPING STUDY



El Paso's existing downtown and I-10 cutting through with the landscaping emphasized.



The downtown if all the slope paving is replaced with landscaping turning I-10 into a green zone.

LANDSCAPING STUDY



If the landscaping is restricted to the tops of the slopes and the flat areas it is still effective.



The leftover slope paving provides a surface for pattern banding of rock aggregate.

DOWNTOWN



Landscaping already placed on top of the slope paving downtown serves as a good precedent. Placed on the flat brow of the slope, it could be helped by treating the rest of the slope paving with rock aggregate as well.



The taller plants have an appropriate scale for the site and more of them would help add height and volume to the planted areas.



The banding of different rocks and plant material creates a very powerful dynamic scene.

LANDSCAPING PRECEDENTS



The variation in low plants and rocks creates textural interest, but more height is required to make an impact at the scale of the site.



The level changes with flat areas and walls create a sense of space as does having lower shrubs and taller trees, more trees would help increase the feeling of space.



The movement created by the banding and terracing transforms the space from a leftover wedge into an interesting parklike green space.

Designation: Railroad
Proposed Color: Blue
Priority for Redevelopment: Current
Estimated Cost: \$ 1.8 M

RAILROAD ARCHES

SITE COLLAGE



The existing arches frame the railroad behind them. Unfortunately, the rusty steel detracts and the lighting feature no longer works. Recent attempts at adding landscaping along the fence provide improvement, but do not solve the issues. If the arches are to be re-illuminated they must be replaced.



Installing a new system of arches, railings and brick supports provides another lifetime of enjoyment to this identity feature. Now the railings match the railroad motif found nearby on the railings at Cotton. Updating the lighting to be more current and sustainable with LEDs activates the arches once again. The adjacent landscaping, not illustrated, should be planted to standards of the I-10 corridor landscaping concept.

Designation: Retail District
Proposed Color: Red
Priority for Redevelopment: 10-20 Year Plan
Estimated Cost: \$ 10 M

HAWKINS

SITE COLLAGE



Hawkins marks the beginning of the series of interchange improvements along I-10. Hawkins prominently marks the Cielo Vista Mall, a major retail shopping destination in El Paso. Aging and in disrepair, the interchange is cluttered and barren, devoid of any aesthetic amenities. It does not support the redevelopment found at the adjacent sites. New hotels and businesses are locating on both sides of the interstate, creating increased demand for pedestrian activities within traffic circulation patterns. This site is ripe for redevelopment.



Hawkins represents the flagship of the design scheme, illustrating the set of aesthetic enhancements proposed for all of the interchanges located along I-10 in retail districts. These enhancements include: light columns, decorative grills, column wraps, amenity lighting, decorative paving, concrete staining, landscaping, and gravel surfacing of abutment walls and slope paving. Each site along the alignment receives an assigned color. Hawkins is designated as red because it is a major retail hub and red is a common color in retail branding.

Designation: Native Landscape
Proposed Color: Earth Tones
Priority for Redevelopment: 10-20 Year Plan
Estimated Cost: \$ 12 M

EXECUTIVE

SITE COLLAGE



Executive has a natural landscape approach that could be very successful but can seem unkempt in such a large area. Adding more plants in the natural palette will help this.



The introduction of more plants fills the space and it is further defined by a series of short walls and rock ground cover banding that organize the space while allowing it to still seem natural. Locating large identity towers at each corner of the bridge defines the gateway into the city.

EXECUTIVE

SITE COLLAGE



The slope paving in the middle of the highway near Executive is rough and barren. It does not fit into its context.



Replacing the slope paving with local gravel in large diagonal swaths adds texture and color tying the slope back to its surroundings.

Designation: Retail District
Proposed Color: Green
Priority for Redevelopment: 10-20 Year Plan
Estimated Cost: \$ 10 M

GERONIMO

SITE COLLAGE



Geronimo marks Basset Place, a retail destination as the biggest shopping center nearby after Cielo Vista Mall. The infrastructure is not integrated with its site. While the infrastructure is in relatively good shape, it is bland and barren, not contributing to the active life of the surrounding community. Only a few blocks south from this I-10 interchange, well-kept single family homes abound. Geronimo could be an asset to its neighbors, if it were to receive a major makeover, including enhanced pedestrian circulation.



Proposed enhancements include: light columns, decorative grills, column wraps, amenity lighting, decorative paving, concrete staining, landscaping, and gravel surfacing of abutment walls and slope paving. Geronimo is designated as green, reinforcing its potential as a landscaped site. This interchange has the largest amount of slope paving that could be removed and redesignated as native landscape. Geronimo has twice as much slope paving compared to other interchanges.

Designation: Commercial/Industrial
Proposed Color: Yellow
Priority for Redevelopment: 10-20 Year Plan
Estimated Cost: \$ 20 M

TROWBRIDGE

SITE COLLAGE



Trowbridge marks a transition point, beginning the next sequence of interchanges. It is one of the largest interchanges in the project scope. Primarily set in a commercial/industrial area, with both smoke stacks and modest homes nearby, it presents a unique opportunity. It combines a variety of wall types and several rows of columns, commanding a dominating view. Currently, it is generic and undistinguished except for its massive footprint. It could be transformed in a variety of ways, becoming more integrated with its neighborhood and site.



The proposed painted application for this site features traditional Mexican tile patterns and bright colors that enliven the area and transform its generic appearance. Proposed enhancements include: painted graphics, decorative grills, column wraps, amenity lighting, decorative paving, and concrete staining. There may be opportunities for landscaping and gravel surfacing of slope paving further away from the interchange, as the project extends to the gore, creating more integration with the other solutions throughout the corridor.

Designation: Commercial/Industrial
Proposed Color: Terra-Cotta
Priority for Redevelopment: 10-20 Year Plan
Estimated Cost: \$ 15 M

PAISANO

SITE COLLAGE



Paisano is similar to nearby Trowbridge. It occupies a large footprint in a primarily commercial/industrial area. This hard-scape site has enormous walls and sloped shoulders. Many of the slopes are steep. Like Trowbridge, it has series of columns that are both an opportunity and a challenge. In its present state it is structurally sound but dingy. There is no human scale. Pedestrian circulation is minimal and dangerous. This site requires a makeover, similar to Trowbridge. Together they anchor the industrial/commercial core.



Paisano and Trowbridge proposed as a pair, both feature a painted graphic application. Paisano is treated with traditional, bold geometries in terra-cotta inspired earth tones. Proposed enhancements include: painted graphics, decorative grills, column wraps, amenity lighting, decorative paving, and concrete staining. There may be opportunities for landscaping and gravel surfacing of slope paving further away from the interchange, as the project extends to the gore, creating more integration with the other solutions throughout the corridor.

Designation: Residential/Commercial
Proposed Color: Red Violet
Priority for Redevelopment: 10-20 Year Plan
Estimated Cost: \$ 10 M

CHELSEA

SITE COLLAGE



Chelsea is grouped with Trowbridge and Paisano because of its proximity. In character, it is closer in feeling to Copia and Piedras. Chelsea presents another transition point, setting the tone for the next series of amenity enhancements with a more residential focus. Chelsea is smaller in scale and well kept. It features huge slope-paved shoulders with ample room for re-design as landscaped terraces, similar to those at Copia and Piedras. Integrating Chelsea with its neighborhood, through enhancements, would greatly improve the entire community.



With bus stops, retirement homes, single family dwellings and small businesses nearby, Chelsea presents a strong candidate for a makeover. The community would immediately benefit from site improvements that include pedestrian amenities. Proposed enhancements include: decorative grills, column wraps, amenity lighting, decorative paving, concrete staining, landscaping, and gravel surfacing of abutment walls and slope paving. Within this series, color choices are tonal, yet distinct for each location, providing clear identity with place.

Designation: Residential/Commercial
Proposed Color: Yellow Ochre
Priority for Redevelopment: 10-20 Year Plan
Estimated Cost: \$ 20 M

COPIA

SITE COLLAGE



Copia represents one of the more successful bridges in the I-10 corridor study. Tonal colors, inclusion of trees, landscaping with rock seeding and mortared surfacing, and replacing concrete slope paving is transforming. Enhancements at Copia, Raynor and Piedras have endured over time, illustrating a proven solution for removing or re-surfacing slope paving. Slope paving dominates and detracts from the I-10 corridor. Here, mature trees soften the site. Terracing provides scale. Pedestrian paths provide connectivity with the neighborhood.



While Copia represents one of the most successful renovations along the I-10 corridor study area, in terms of use of color and landscaping, there is still room for improvement. Proposed enhancements include: decorative grills, column wraps, amenity lighting, decorative paving, concrete staining, landscaping, and gravel surfacing of abutment walls and slope paving. Solutions at Copia must reinforce the amenities of the site and to add to them. Proposed renovations will support pedestrian circulation and greater connectivity with neighborhoods.

Designation: Residential/Commercial
Proposed Color: Burnt Sienna
Priority for Redevelopment: 10-20 Year Plan
Estimated Cost: \$ 10 M

PIEDRAS

SITE COLLAGE



Piedras presents a companion site to Copia. Both bridges feature tonal colors, the inclusion of trees, landscaping with rock seeding and mortared surfacing, and replacing concrete slope paving. This is transforming where it is applied. Not all of the sloped shoulders at Piedras have been landscaped. This site needs finishing and updating similar to Copia. It has residential and commercial neighborhoods flanking the site with ample room for site improvements, including the addition of bicycle paths.



While Piedras represents a successful renovation along the I-10 corridor, similar to both Copia and Raynor, there is still room for improvement. Proposed enhancements include: decorative grills, column wraps, amenity lighting, decorative paving, concrete staining, landscaping and gravel surfacing of abutment walls and slope paving. The aesthetic amenities at Piedras get added to, in order to finish and reinforce them. Proposed renovations support pedestrian circulation and greater connectivity with nearby neighborhoods.

Designation: Railroad
Proposed Color: Earth Tones
Priority for Redevelopment: 10-20 Year Plan
Estimated Cost: \$ 8 M

COTTON

SITE COLLAGE



Cotton represents a unique situation on the I-10 corridor and demands a unique solution. It dominates the surrounding neighborhood with a forest of columns. These columns provide the most interesting opportunity for aesthetic enhancement.



Painting the columns in patterns redefines the highway's underside breaking up the visual monotony of the space. Continuing the railings with their locomotive motif ties the highway to Cotton's bridge below. Treating both I-10 and Cotton similarly unifies the site strengthening its visual and spacial message.

SUMMARY OF DESIGN GUIDELINES

I-10 creates a spine through El Paso connecting the City's many neighborhoods along its length. Roadway interchanges and bridges mark each neighborhood. These bridges, whether they go under the highway, as in the neighborhoods to the east, or span over the highway, as in the downtown and to the west, serve as opportunities to represent the identity of the neighborhoods they connect. Achieving this goal requires the implementation of a comprehensive aesthetic plan including specific artistic elements. Unifying like elements makes each bridge a part of a larger scheme.

This book provides design guidelines for existing and future developments along the I-10 Corridor and serves as a reference for future roadway developments in the El Paso region. All applications are site responsive and require individual design development.

Slope Paving: eliminated in future projects; removed and replaced with rock aggregate and landscaping in existing projects where possible

Landscaping: Minimally 3 shade trees & 5 ornamentals per 1,000 sqft for gores
50 trees per acre for open space

Medians: remove hardscape; create a landscape plan with native plants and rock aggregate include identity elements

Neighborhood Identity: Use aesthetic design elements to create a sense of place

Facades: add screens inspired by local patterns and neighborhood character

Columns: add aesthetic treatments; promote pedestrian scale

Lighting: add pedestrian lighting and amenity lighting for wayfinding

Walkways: add patterns and textures; promote traffic calming

Abutments: clad with rock aggregate or paint abutments

Walls: apply penetrating stain to existing; eliminate paint and promote relief for future projects

Railings: create aesthetic railings inspired by local patterns and neighborhood identity

The City and its Agencies, with guidance from the Master Plan Design Team, determined this priority for the implementation of the design guidelines:

Airway: current

Downtown: current

Railroad Arches: current

Hawkins: future / first priority

Executive: future / second priority

Geronimo: future

Trowbridge: future

Paisano: future

Chelsea: future

Copia: future

Piedras: future

Cotton: future

When possible, prioritize aesthetic improvements with neighborhood redevelopment. This may modify the order of implementation; take advantage of site developments as they occur over the next 15-20 years.

CONCLUSION

NOTES

While the three top priority sites; Airway, Downtown and the Railway Arches; are currently scheduled to receive implementation of the aesthetic improvements described in this book, the process of transforming the I-10 corridor will take time. Over the next ten to twenty years the recommendations made here must be implemented incrementally, building toward the total plan. Each site represents an opportunity to take a step towards this larger goal. By applying this plan to each site, over time, they all will be brought to the same level and eventually the whole corridor will feel complete. This goal must always be in mind when deciding and designing the future of the I-10 corridor. Each site must be addressed specifically within the larger plan to create neighborhood identity that fits into the regional context. El Paso and its communities will benefit greatly from this sort of long term vision with specific focus on neighborhood identity. The plan also integrates native sustainable landscape with the infrastructure of the city tying it back to its regional terrain. All these elements, when combined, improve quality of life of the city and present a statement of forward thinking to those using the I-10 corridor traveling to, from and through El Paso.

Images Credits

Page 17: Column 1 Lower Image; <http://www.traderscity.com/board/userpix29/22687-river-gabion-mattress-hexagonal-double-twisted-mild-steel-wire-mesh-1.jpg>, Column 2 Lower Image; <http://www.ieca.org/imagesPhotogallery/george9.jpg>, Column 3 Middle and Lower Images Google Street View

Page 23: Column 1 Upper Image; Pima County Art Program, Middle Image; Cliff Garten, Column 2 Upper and Lower Images; Sound Transit Art

Page 24: Column 1 Upper Image; http://www.greenvilledailyphoto.com/wp-content/uploads/2007/09/20070925_Linky_Stone_Park_2_600x400.jpg, Column 2 Upper Image; <http://media.lonelyplanet.com/lpimg/24163/24163-1/preview.jpg>, Middle Image; flickr user smohundro

Page 26: Column 1 Upper Image; Cliff Garten, Column 2 Lower Image; http://1.bp.blogspot.com/_YGN3l86nWKw/TJAU96Kz_pl/AAAAAAAAFwo/wPTRXlg41jU/s1600/Banksia+mural+2.JPG

Page 28: Column 2 Upper Image; Mineral Life

Page 29: Column 1 Lower Left Image; Shiela Klein, Column 2 Upper Left Image; Laurie Lundquist, Upper Right Image; http://www.lightplan.net/images/Perth_Eastern%20Gateway_300small.JPG, Lower Image; Tillett Lighting Design Inc.

Page 30: Column 2 Lower Image; Philips

Page 33: Column 1 Upper Image; Steven Weitzman

Page 35: Column 1 Upper Image; Barbara Gryguts, Middle Image; Nobi Nagasawa, Lower Image; Carolyn Braaksma
Column 2 Lower Image; Mitsu Overstreet

Page 36: Column 1 Upper Image; Cliff Garten, Middle Image, Carolyn Law, Lower Image; Barbara Grygutis,
Column 2 Upper Left Image; Barbara Grygutis, Lower Image; Tuscon Artists

All other images from VS SiteWorks and El Paso team: Vicki Scuri, Alex Polzin, Yvianna Hernandez, and Brenda Perry.
Cost estimates provided by Jacobs Engineering, Dallas.